

Mosman Park Town Centre Area

Character and place analysis and draft vision

*Let's
Revitalise*

MOSMAN PARK'S
TOWN CENTRE

SHARE YOUR VISION!

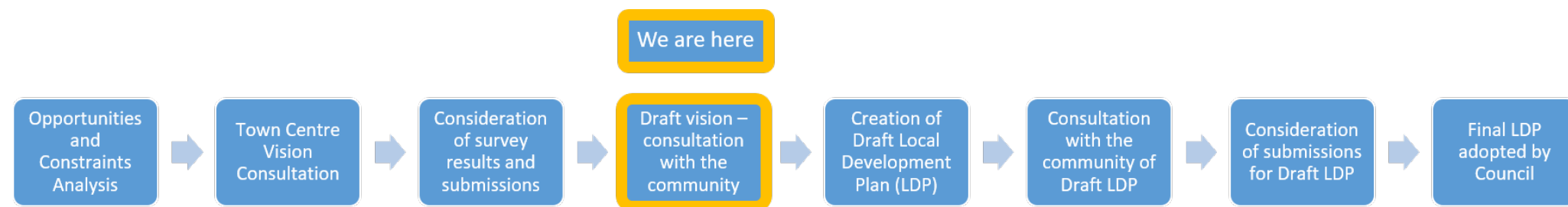


TOWN OF
MOSMAN PARK

mackay urbandesign



These consultation workshops are part of a multi-stage process that began last year.





Community consultation provided a range of comments:

Built Form

- Buildings should create a sense of place
- Buildings next to Heritage buildings should have limited height limits
- Sympathetic materials should be used on new buildings consistent with the established character
- Landscaping should be required
- Sustainable designs / use solar panels etc.
- Ensure the street is activated – design frontages to increase interaction
- Keep facades of existing buildings

Movement

- Better parking management / provision
- More cycling infrastructure
- Improved lighting for better pedestrian safety
- A new and improved crossing / better connection between east and west sides of Stirling Highway
- Better school traffic management
- Oppose the widening of Stirling Highway

Public Realm

- Centralised public meeting place / Piazza / Town Square
- Rooftop gardens
- More street trees with a selection of species
- Improved landscaping
- Public art
- Better lighting / festoon lighting
- Matching street furniture
- Wayfinding signage
- More alfresco
- Bike hire schemes
- Coordinated landscaping
- Markets / Festivals / Events
- Design public spaces so that these can be held at any time
- Safety considerations (footpath and road design).

Character

- To develop a 'style guide' for the area using local characteristics
- Use beach and nature as colour scheme
- Retain heritage and character buildings
- Incorporate heritage and character aspects into new buildings

Positives	Negatives
<ul style="list-style-type: none"> • Parking • Has a village feel • Walkable • Heritage / character • Church precinct • Alfresco • Good mix of land uses • Vintage shops • Night time economy • Relaxed feeling 	<ul style="list-style-type: none"> • Parking • Not enough bike infrastructure • Too many side exits for cars • Difficult to cross SH • School traffic • Dingy laneways • No 'heart' • Flats look run down • No clear character • Too many chain stores • Not enough street trees/shade

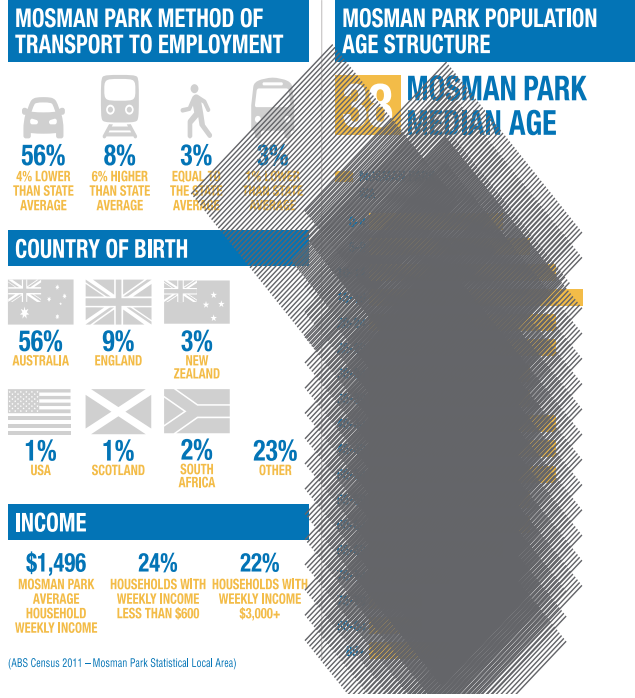
Summary

Respondents appreciated the scale of the town centre and the traditional elements that lend character to the place. In regard to scale, the appeal was that it was a 'village', in that it was large enough to have good things, but not so large that the good things were overwhelmed by the size and busyness of the place. On the down side, the part of the town centre (particularly the western part near Stirling Highway) is perceived as disjointed, unappealing and unsafe, and lacks a distinct 'there' when you get there.

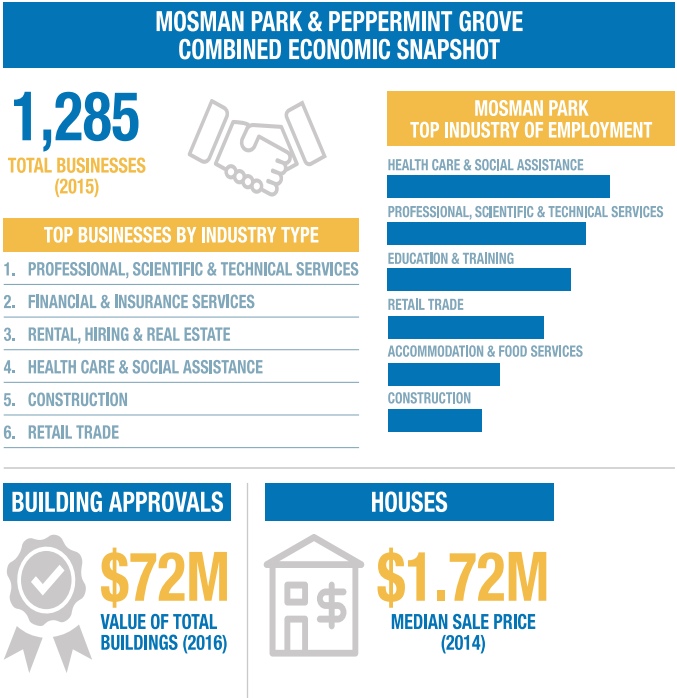
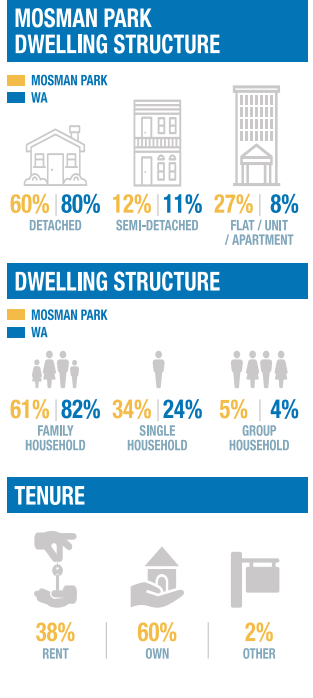
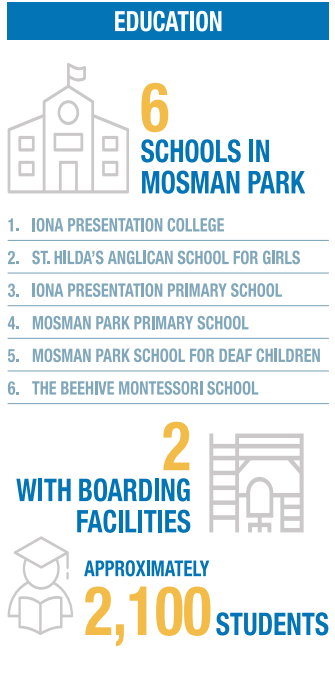
What we previously learnt about the social and economic profile

The first part of the study was undertaken by Roberts Day in 2017, which included analysis, broad recommendations and community consultation, summarised as follows:

Social Profile



Economic Profile



Summary: Mosman Park is largely representative of Perth's middle suburbs - diverse and family-orientated - but better connected, generally wealthier and better educated.



Activities

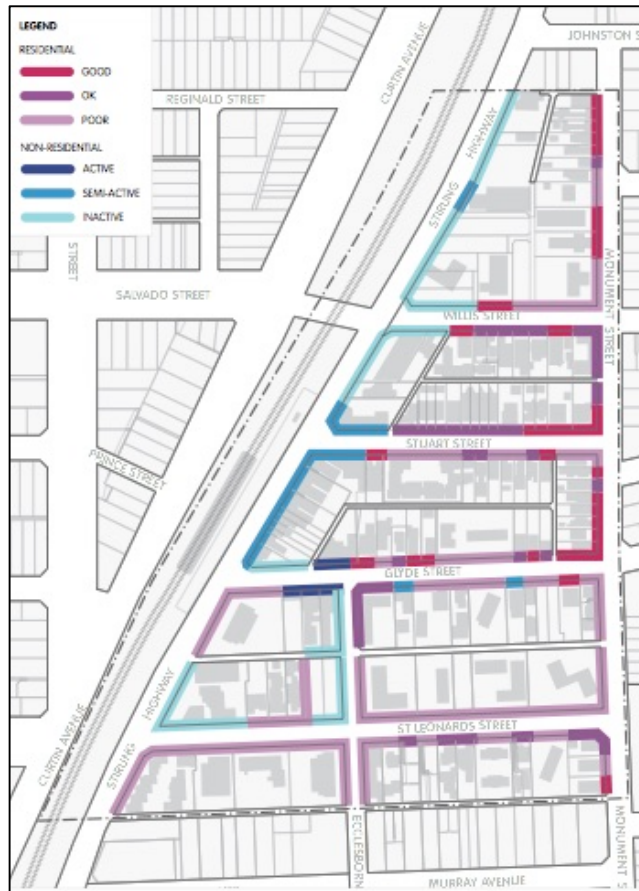


Streetscape qualities

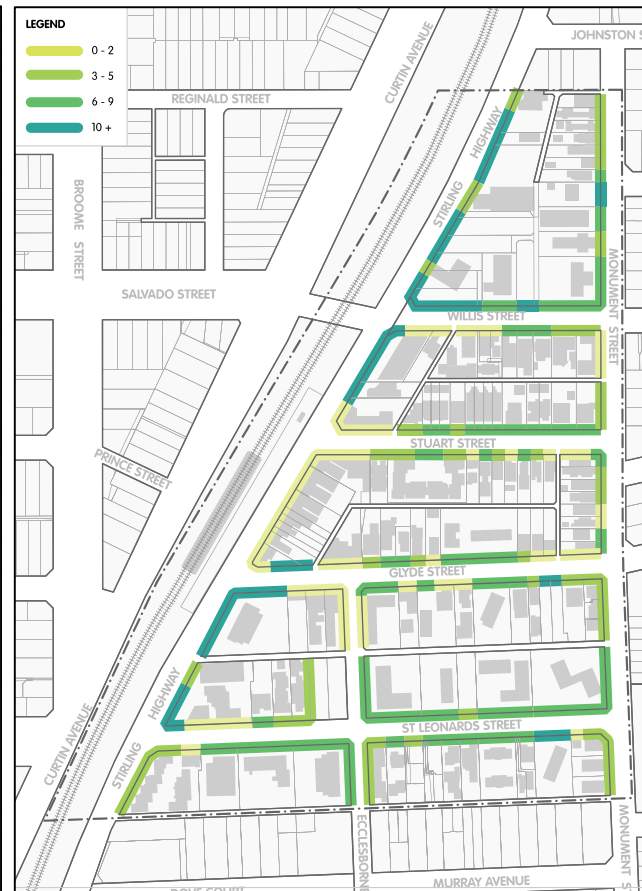


Zoning

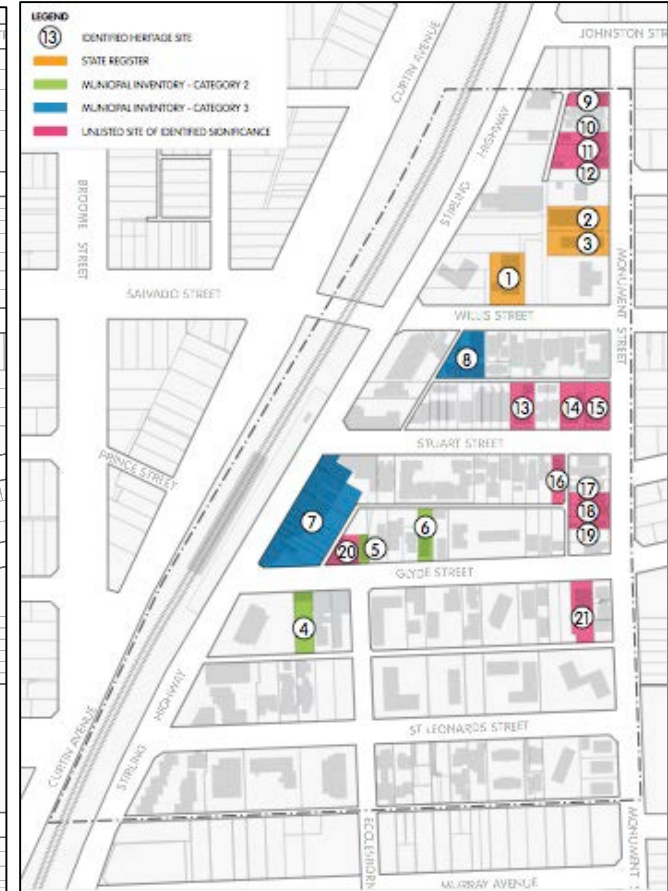
Summary: There is a surprising amount of activity in the town centre precinct but in a streetscape that generally leaves room for improvement, and a planning framework that enables significant capacity for change.



Building frontage



Setbacks



Heritage

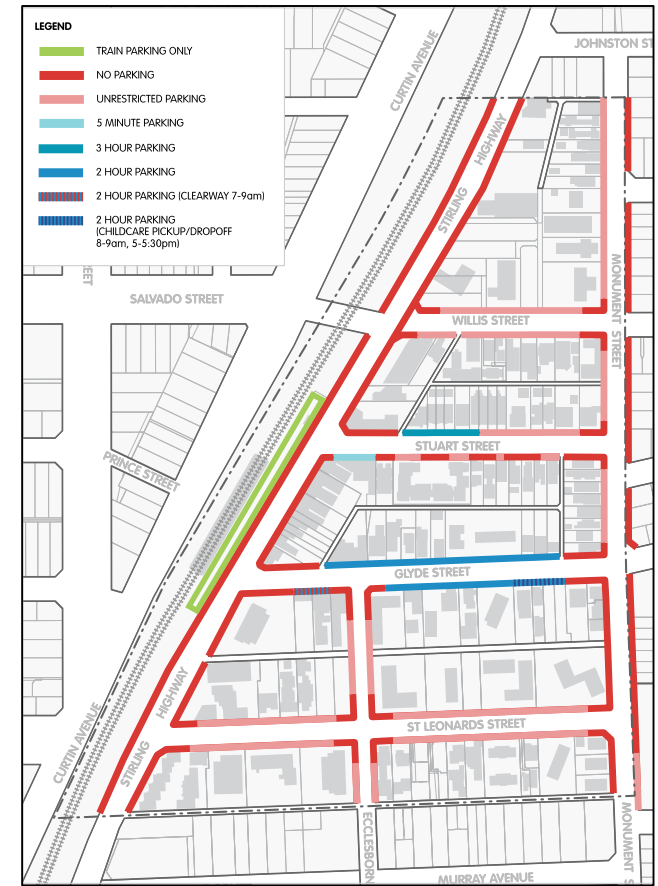
Summary: Much of the town centre precinct presents well with ‘goldilocks’ setbacks (not too small, not too large) but deteriorates rapidly when it meets Stirling Highway. Surprisingly, for a place with a clearly traditional character, there are only a few listed buildings.



Open space



Tree canopies



On-street parking

Summary: Whilst there are lots of trees, there are few parks, which means that the trees are generally either in the street or on private property. Also, for a traditional town centre precinct, there's not a lot of on-street parking, which has perhaps contributed to the large swathes of off-street bitumen.

Summary: Land use and spatial planning

Whilst the planning framework is in place to enable redevelopment, additional effort is required to deliver not just an outcome, but the best possible outcome.

The main area of focus for further encouragement and guidance is the Stirling Highway interface and the laneways behind.

Additionally, the role of the town centre precinct needs to be clearly different, and complementary to, the shopping centre on Monument Street.

Summary: Buildings and urban form

New development should respond to the character of the existing place that is derived from the existing heritage and character building, whilst providing more opportunities for new businesses and better pedestrian amenity on the streets.

Summary: Public spaces and environment

Because there is little in the way of parkland, the landscape in the streets has to do much of the heavy lifting when it comes to creating a lush and more appealingly green environment.

Summary: Access and transport

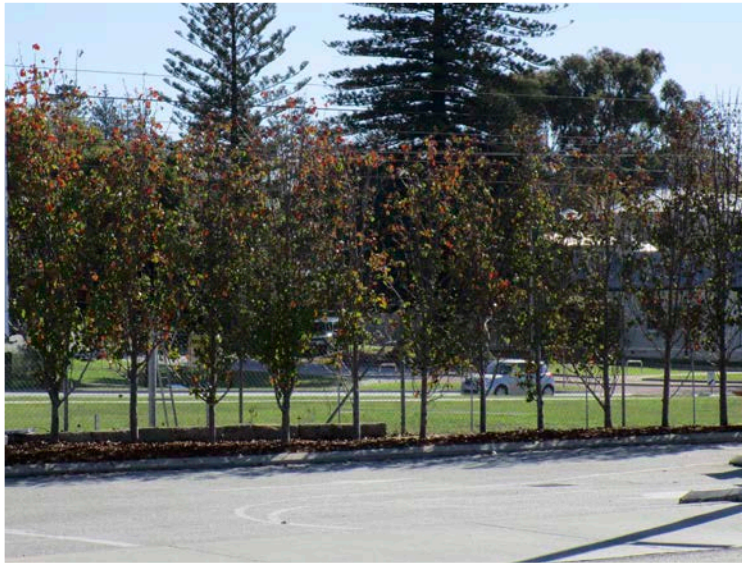
The focus needs to be on improving pedestrian infrastructure to make walking a more convenient and pleasant alternative to car use. At the same time, the streets need to carry more of the parking load, and do it efficiently, so that the fabric of the town centre is not riddled with car parks.

Summary: Place-making and activation

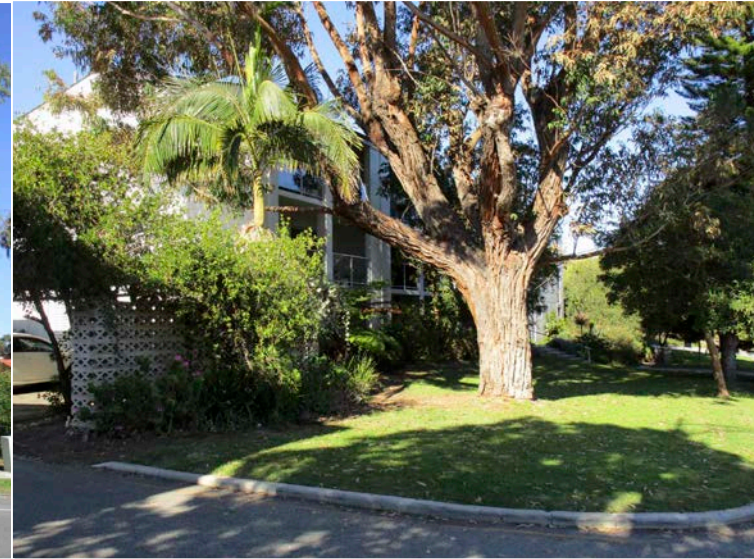
Whilst improving the fabric of the town centre precinct (the buildings, the streets, the landscape) is important, there is also plenty opportunity to increase the number of things for people to do in the town centre.

The Town of Mosman Park subsequently engaged Mackay Urbandesign with Propagule landscape architecture to and landscape architect to undertake an assessment of the town centre area from a character perspective. The analysis considered:

- **Strengths and Weaknesses**
- **Architectural Character**
- **Built Form**
- **Architectural details**
- **Materials & colours**
- **Pedestrian Amenity**
- **Climate Responsiveness**
- **Streetscapes**



Trees planted in rows that, regardless of the species, become a defining feature of an urban landscape rather than random remnants of a previous natural landscape.



Mature 'hero' trees, in generous front setback areas, that are tall enough to dwarf most of the buildings and remind us of the majesty of the natural world.



Perfect street interfaces – generously and interestingly landscaped front setbacks, low fences for passive surveillance and neighbourliness, footpaths for pedestrian convenience and street trees for shade and shelter, on-street parking and no crossovers.. It doesn't get much better than this.



Simple streets with street trees and footpaths on both sides of the street.



Verges planted with something more interesting than just turf.



Buildings with simple forms that are homely, recognisable, and allow the landscape to shine in the foreground.



Likewise, and again with the use of open fencing that allows the landscaping in the front setback to be seen and contribute to the streetscape.



Laneways with a well-contained edge and enough planting to make it a an attractive space that goes beyond being merely utilitarian.



Laneways edged with housing and some landscaping that transforms them from a utilitarian space for garage and bin access, to a 'mews' or mini street.



Whilst they may not be from people's favourite architectural era, the apartment buildings in the southern part of the town centre precinct are unfussy, avoid 'try-hard' architectural tricks, and utilise rhythms and proportions that are derived from traditional architecture.



The long and the short of it - Mosman Park has a diversity of buildings that makes it more interesting than the average suburb.



Centenary Park –something of a hidden gem, just in need of a little polishing.



An example of a corner being seen as something to be celebrated with a larger building that has not one but two public faces, and twice as much frontage to landscape.



Anywhere else, the engineers would have concreted the corner, or it would have been left as a dry patch of weedy turf. The use of groundcover helps to make the corner special.



Beautiful houses deserve beautiful fences.



Even the simplest of apartment buildings can become elegant contributions to the streetscape with a good fence and/or hedge.



Since when have blank walls been considered as a strength of the urban environment? Answer: when they are viewed as blank canvases for public art. Go forth and paint ...



The welcoming gesture at Glyde Street's 'front door' to the town centre is the end wall of an old apartment building and a fenced-off car park.



The other side of Glyde Street is not much better with the back wall of a building and a small open car park.



The car parking to the rear of the Stirling Highway shops is a large 'hole' in the northern edge of Glyde Street.



Whilst the town centre as a whole has a leafy character, the Glyde Street heart is an over-exposed ribbon of bitumen and brick paving.



Stirling Highway is an unpleasant experience with pedestrians having to walk close to moving traffic. At least in this section, there is some shade and shelter.



Stuart Street has the bones of a good retail street with businesses on both sides of the street and convenient on-street parking. But ... it is in desperate need of trees for shade and character.



The station is an important pedestrian destination but has a double moat of bitumen between it and the town centre – Stirling Highway and a car park.



Infrastructure such as bus stops, phone booth, seats, bins, etc look as if they've been dropped into place randomly rather than being integrated into the streetscape.



Stirling Highway is even more forbidding for pedestrians in the northern part of the town centre precinct, with no shade or shelter to the footpaths.



The car park to the north of IGA is an unsightly swathe of bitumen.



The gaps between the buildings along Stirling Highway expose the garages at the rear of the residential lots behind, adding to the message that is a place for cars not people.



In the northern part of the town centre precinct, pedestrians are sandwiched between moving cars on the Highway, and parked cars or even more moving cars on the adjacent commercial properties.



Is it a laneway? Is it a car park? Whatever it is, it's not a place people would want to be.



Harburn Mews at the boundary between Mosman Park and Peppy Grove. A space without a purpose – it leads from a fast food drive-through to a residential street and is inaccessible by car – and, with little overlooking, a scary space at night.



Willis Street a dead-end street - or it would be if drivers didn't take the shortcut through the service station to overcome the problem of limited access to and from the Highway.



Another laneway that is part laneway, part car park, but definitely not a pedestrian friendly link, which is a pity because it is the quieter alternative to Stirling Highway as a walking route between Willis Street and Stuart Street.



A residential lane in the southern part of the town centre precinct that lacks sufficient containment (whether by trees, buildings, or both) to be a pleasant space.



Welcome to the Mosman Park town centre – if approaching from the south. An uninspiring entry.



Monument Street – an avenue crying out for trees.



A laneway in the southern part of the town centre precinct that has the makings of a pleasant 'mews' street. However, it needs more definition by buildings and trees and somewhere to put the bins.



“It’s landscape Jim, but not as we know it”: an example of land given over for landscape but with no landscape (other than grass) in it.



Ditto.

The character of the built form is derived, in part, from the diversity of architectural style but also from common characteristics found in the buildings ...

Style - eclectic architecture

Examples of the eclectic range of architecture in the Mosman Park town centre include:

- Traditional buildings with a sense of grandeur
- Simple traditional shop buildings
- Traditional cottages and houses
- Late 20th century houses
- Grouped dwellings
- Late 20th century apartment buildings
- Utilitarian contemporary commercial buildings
- Quirky' commercial buildings.



Building forms

The building forms are generally simple, rectangular forms.

The more visually appealing built forms display:

- Pitched roofs
- Repeated elements on the frontage such as gable ends, gablets, and porches
- The use of colour to define larger building elements
- The use of openings ‘punched’ into the wall rather than curtain wall glazing



Roof form

Many of the residential and institutional buildings include large steeply pitched roofs.

The more visually appealing built forms display:

- Gablets with glazing or timber details
- Lighter colour metal or dark red/brown tiles
- Gable ends with overhangs
- Mix of contrast colours for roof details such as gutters and bargeboards.



Details

The residential and institutional buildings displayed subtle details, such as:

- Use of shallow curves for windows and veranda openings
- Horizontal banding, usually in white
- Decorative breeze blocks in post-war buildings
- Vertical timber rails.



Details

The traditional Main Street also has examples of subtle details such as:

- Elegant, simple detailing for traditional shop frontages
- Angled glass entries with inset doors
- Double doors in bright painted colours.



Built form character analysis: Openings

Openings (doors and windows)

The residential buildings, both single houses and apartments display certain characteristics including:

- Vertical proportions
- Large windows are subdivided into smaller panes
- Openings are ordered and repetitive for apartments
- Openings are paired or grouped together on a wall.



Built form character analysis: Material and colours

Materials and colours

Residential buildings display a distinctive palette focused on:

- Wall colours of red and cream, buff brown
- Wall materials of limestone, brick and light coloured painted render
- Roofs that are metal or tile in colours of red, terracotta and grey
- Details in painted timber and weatherboard, including rails for fences.

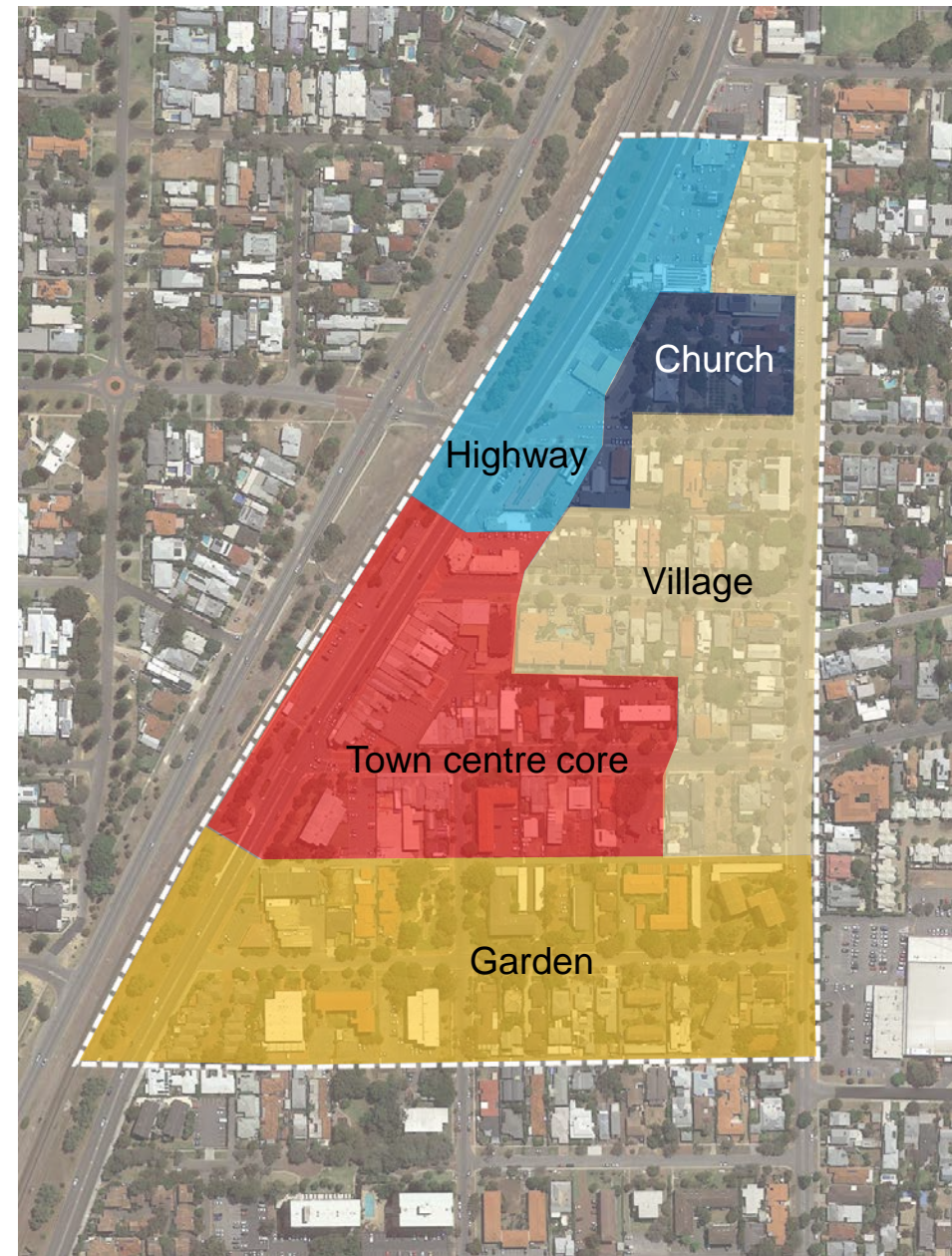


After analysing the overall architecture of the Town Centre Area, it is clear that there are five distinct precincts, each with their own bespoke character.

Precincts

The overall town centre precinct has a number of streets and places with their own unique characteristics. These can be defined as a series of precincts, as illustrated in the adjacent precinct plan.

By identifying the area as a series of precincts, each of the precincts can be governed by its own set of guidelines to help ensure that the uniqueness of each part of the town is maintained rather than applying a 'one-size-fits-all' planning framework to all of the parts and risking the whole place looking alike over time.



Town centre retail core precinct

A low-scale village feel, characterised predominantly by traditional 'Main Street' terraced buildings transitioning to stand-alone buildings at the interface with the adjacent residential area. The built form is characterised by:

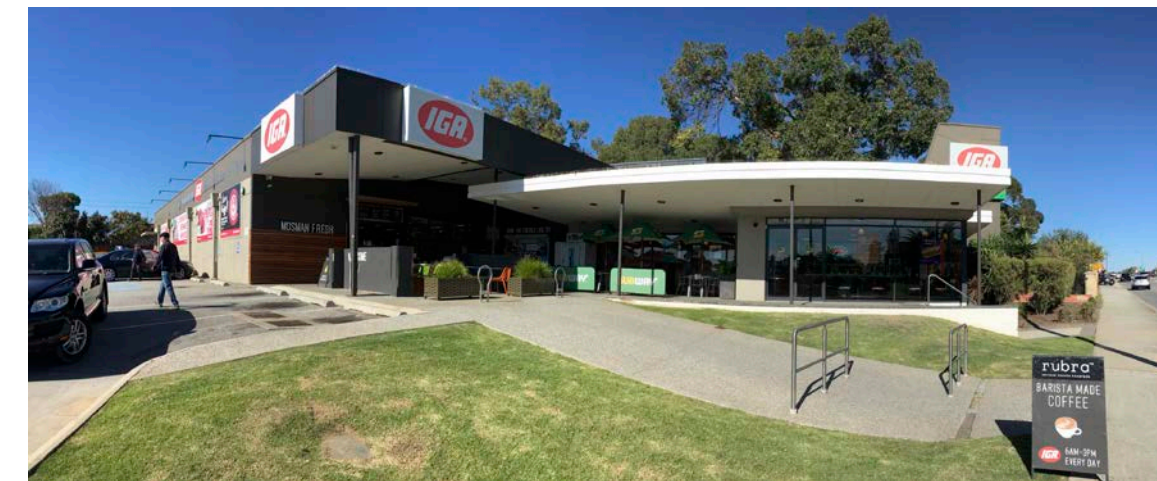
- Simple regular-shaped building forms
- Glazed frontages with ceiling-height shopfront windows with a raised sill and punctured windows
- Slender window detailing in the older shop frontages (not 'chunky' framed windows)
- Roofs hidden behind parapets
- The use of colour, either to pick out individual buildings or to define a short run of buildings
- Signage mostly integrated with canopies and shopfronts rather than on top of buildings.



Town centre highway commercial precinct

A utilitarian and car-oriented feel characterized predominantly by standalone buildings with large setbacks and surrounded by car parks. The built form is characterised by:

- Boxy buildings with flat roofs
- Minimal architectural detail
- Muted colours that allow signage to be emphasised
- Expanses of blank walls and single entry points oriented to car bays.



Village residential precinct

A low-scaled village feel characterized by an eclectic mix of single and grouped residential buildings in well-landscaped streets. The built form is characterised by:

- Terraced single-storey cottages with pitched roofs and verandas
- Town houses with pitched roofs and balconies
- Double-storey or single-storey dwellings with attic rooms
- Modest front setbacks with low height fences and gardens
- Snout garages, high front walls dominate in some streets
- Rear laneways with access to an eclectic mix of rear garages.



Built form character analysis: 'Garden' residential precinct

Garden residential sub-precinct

A medium-scaled garden city feel with apartments in a landscaped setting. The built form is characterized by:

- 'Modern' apartment blocks from the mid-to late 20th century
- Boxy rectangular buildings predominantly three storeys in height (walk ups) with shallow pitched roofs
- Landscaped setbacks that range from modest to generous areas with many larger trees
- Apartments with windows and balconies facing the street
- Large open car park courts generally to the rear of the buildings.



Church sub-precinct

A traditional institutional feel characterised by larger-volume buildings such as churches and halls set in a generous landscaped setting. The built form is characterised by:

- Substantial solid buildings constructed in masonry with steep roofs
- Tall and narrow openings with arched heads
- A palette of limestone, brick and light painted colours
- Front doors that are clearly visible
- Landscaped railing fences that allow landscape to be seen from the street



Built form character analysis: Pedestrian amenity

Pedestrian amenity

The town centre core has a strong built form character that is enhanced by the pedestrian amenity afforded by glazed shop fronts for people to look into and canopies that provide shelter and a sense of enclosure.

Visual appeal is added through:

- Canopies that include decorative struts
- Zero building setback to relate to the pedestrians on the foot path
- Interest provided by glazed punched shop front openings with a raised sill to frame the internal view.



Climate responsiveness

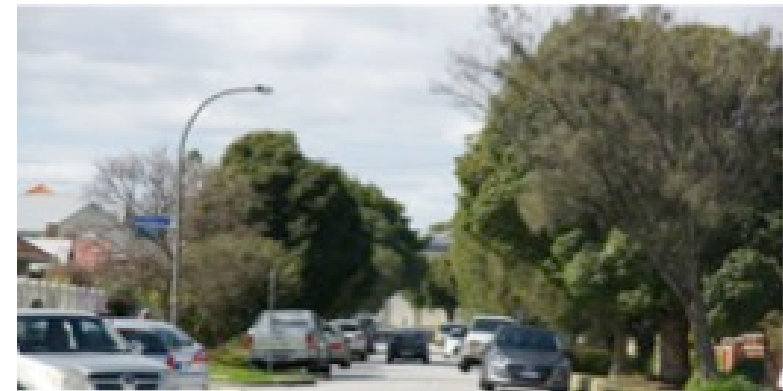
The residential buildings displayed different types of climate control:

- Canopies over windows in different and decorative forms, some with struts
- Wide verandahs on building frontages that offer outdoor living space and protect the front door
- Roof eaves overhangs.



The streetscapes of the Mosman Park town centre and surrounding areas are characterised by:

- An eclectic mix of exotic and native trees in the streets and front setbacks
- Streets with a typical inner-middle ring suburban cross section
- Verge types including irrigated and non-irrigated turf, mulch, groundcovers and paved parking areas, with turf remaining the dominant treatment.
- Ad-hoc treatments of crossovers, even for lots with access to a rear laneway.
- A range of front setback treatments including turf, to cottage gardens, native gardens, and formal gardens, with and without fencing.
- A range of fence treatments including traditional timber picket, metalwork and masonry piers, solid wall and and contemporary batten fences.



The summary conclusions from the on-site analysis are:

- The overall character of the built form is highly eclectic, and a place with an eclectic mix of built forms is best unified through landscape. In short, landscape is the key to greater visual consistency.
- The landscape character is also highly eclectic with a mix of native and introduced garden species, and planting at different scales. An eclectic landscape is a common aspect of residential gardens.

Major theme

*There is a major thematic opportunity of reimagining the Mosman Park town centre area as **'The Gardens'**, where a dense and eclectic landscape character is encouraged to become, in effect, **a habitable botanical garden with great linkages.***

- *Is there anything that has been missed in the analysis?*
- *Do you have any comments on the strengths and weaknesses?*
- *Do you have any additional thoughts on architectural character, built form details or landscape identified?*
- *Do you agree with the division of the area into five districts and are the boundaries correct?*
- *Do you agree with the character identified for each area:*
 - *Town Centre*
 - *Highway Commercial Precinct*
 - *Village Residential Precinct*
 - *Garden Residential Precinct*
 - *Church Precinct*
- *What are your thoughts on the major theme for the precinct as a botanical garden?*

Building on the idea of buildings within a habitable botanical garden as the major theme, there are four groups of initiatives that are proposed to revitalise the town centre area:

- **Landscape and streetscape initiatives:**
 - **More trees and landscaping to provide greenery, shade and a more pleasant environment for pedestrians**
 - **Emphasise a hierarchy of streetscapes that relate to the five identified precincts**
 - **Entry statement initiatives**
- **Place-making initiatives – Quick wins**
- **Place-making initiatives – Longer-term proposals**
- **Bespoke design guidelines for each of the five precincts so we get appropriately characterful buildings that respect their place.**

Landscape Concept plan



Landscape vision

To support the idea of the Mosman Park town centre being a visually appealing habitable botanic garden.

Key principles of the Landscape concept plan

- Establish Stirling Highway as a landscaped 'front door' to the town rather than a de-facto bypass.
- Strengthen the landscape quality of the streetscapes.
- Adopt a 'gold, silver and bronze' paving and infrastructure palette to visually reinforce the importance of the town centre.
- Retain existing trees where possible and augment with new planting of varied species for visual interest and diversity.
- Encourage flowering plant species and species with varied forms to provide further visual interest and diversity.
- Introduce planters and climbers to raise the landscaping from the floor.
- Use of ground covers and low shrubs to visually soften the streetscape and contrast with vertical elements.
- Mandate Street setbacks with deep soil (outside the town centre core) sufficient to support new planting of introduced and native trees.
- Use deciduous trees to enable solar penetration in winter
- Introduce rain gardens where possible to harvest stormwater and infiltrate it locally.

Landscape principles

- Embrace the diversity of trees and other planting, celebrate the diversity and augment it.
- Utilise a diverse landscape as a dominant theme to pull together the differing characters of the built form in the different precincts
- Utilise the diversity of landscape to enable broader bio-diversity and greater awareness of seasonality
- Encourage a colourful landscape palette to add life, fun and vibrancy to the place, especially where the architecture does not.



Street trees

The proposed street tree palette provides for a range of native and introduced species that continues the theme of diversity, and includes trees that respond to the different needs in urban and residential streets such a shade, colour and punctuation.

Street Tree Palette



Street tree selection principles

- None of the streets are formal boulevards or avenues
- Streetscapes are currently quite eclectic and varied in character
- Residents have some flexibility as to replacement species when a tree dies
- Mixture of deciduous trees where winter solar access is preferred
- Evergreen flowering trees that provide seasonal interest and support habitat opportunities
- Potential to phase out certain species over time that are less favoured like the Queensland Brush Box (*Lophostemon confertus*)
- Smaller ornamental species in alfresco areas to create enclosure and textural interest
- Majestic but manageable specimens with interesting features i.e. trunk/ bark/ flowers/ foliage/ texture
- No large specimens that will create overshadowing, limb drop or root invasion issues in future.

Landscape and streetscape initiatives: Planting palette

General landscape palette

The general landscape palette provides for a common base planting theme for different understorey locations in the public realm.

The general landscape palette:

- Establishes a reference to the underlying geomorphology of the Swan coastal plan
- Provides a degree of 'common DNA' throughout the town centre area
- Creates a starting point for further localised augmentation with other species
- Provides a high degree of visual interest and diversity of colour and form
- Recognises spaces of different uses and qualities such as car parks, rain garden, sunny and shady spaces.



The paving and infrastructure elements are tiered into three palettes – gold for the town centre core, silver for the town centre frame, and bronze for the surrounding areas.

Gold-Silver-Bronze palettes
(relative to the town centre)



Gold Standard Streetscape (Glyde Street)



Silver Standard Streetscape (Stuart Street)



Bronze Standard Streetscape (Willis Street)

Paving palettes

The gold silver and bronze palettes for the paving treatment are selected to visually reinforce the relative importance of the Mosman Park town centre core and then graduate its qualities back into the surrounding area.

Specific qualities of the paving palettes include:

- A complementary colour palette
- Suitability for simple detailing and high levels of workmanship
- Low embodied energy
- High durability
- Low maintenance requirements
- Recyclable materials
- Natural local material

Gold Standard Finishes

Mass Paving | Insitu Concrete



- Name: Exposed Aggregate Concrete
- Finish: Washed aggregate + saw cut joints
- Size: Saw cut joints at 2m centres
- Colour: Holcim Gestone | Magnolia
- Location: Footpaths, Medians, Crossovers
- Typical Rate: \$120/m²

Feature Paving | Stone Unit Paving



- Name: Porphyry Cobbles
- Finish: Exfoliated / Non-slip finish
- Size: 60 x 60 x 60 thick (split)
- Colour: Mixed / Natural Brown + Grey
- Location: Bandings, Thresholds + Nodes
- Typical Rate: \$290/m²

Tactile Indicators | Stainless Buttons



- Name: Tactile Ground Surface Indicators
- Finish: 316 Stainless Steel, 40% contrast
- Size: 600 x 35 x 20
- Colour: Brushed stainless (silver)
- Location: Crossings, Ramp landings, Hazards
- Typical Rate: \$900/m²

Gathering Nodes | Composite Decking



- Name: Composite Decking & Batters
- Finish: Close butted with steel edge
- Size: 140 x 26 Composite Planks
- Colour: Warm, dark tone
- Location: Formal Gathering Nodes + Ramps
- Typical Rate: \$600/m²

Silver Standard Finishes

Mass Paving | Coloured Insitu Concrete



- Name: Cream Coloured Concrete
- Finish: Sponge finish + saw cut joints
- Size: Saw cut joints at 2m centres
- Colour: Holcim Pumice
- Location: Footpaths, Medians, Crossovers
- Typical Rate: \$90/m²

Feature Paving | Concrete Unit Paving



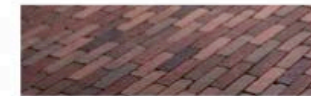
- Name: Borac Bradstone Cobbles
- Finish: Tumbled-look / Non-slip finish
- Size: 450 x 300 x 60 (5 cobble texture)
- Colour: Granite (Mottled Cream)
- Location: Bandings, Thresholds + Nodes
- Typical Rate: \$120/m²

Tactile Indicators | Blade Shaft



- Name: Tactile Ground Surface Indicators
- Finish: Polyurethane, 40% contrast
- Size: 600 x 35 x 20
- Colour: Black
- Location: Crossings, Ramps, Hazards
- Typical Rate: \$150/m²

Gathering Nodes | Brick Paving



- Name: Ceramic Brick Unit Paving
- Finish: Close butted with concrete haunch
- Size: 120 x 20 Planks
- Colour: Warm, dark tone
- Location: Semi-formal Gathering Nodes
- Typical Rate: \$90/m²

Bronze Standard Finishes

Mass Paving | Natural Insitu Concrete



- Name: Natural Concrete
- Finish: Streamed finish + ruled joints
- Size: Saw cut joints at 2m centres
- Colour: Holcim Magnolia
- Location: Footpaths, Medians, Crossovers
- Typical Rate: \$60/m²

Feature Paving | Stamped Insitu Concrete



- Name: Stamped insitu concrete
- Finish: Rock salt, drag textured / Non-slip finish
- Size: 90 x 120-60 x 100 thick (stencilled)
- Colour: Charcoal
- Location: Bandings, Thresholds + Nodes
- Typical Rate: \$90/m²

Tactile Indicators | FRP Tiles



- Name: Tactile Ground Surface Indicators
- Finish: Fibre Reinforced Polymer, 30% contrast
- Size: 600 x 600 x 20
- Colour: Black
- Location: Crossings, Ramps, Bus stops
- Typical Rate: \$75/m²

Gathering Nodes | Stabilised Gravel Fines



- Name: Terrabond stabilised Summerstone Fines
- Finish: Terrabond stabilised / Compacted
- Size: 5mm including fines
- Colour: Pale Cream / Beige
- Location: Informal Gathering Nodes
- Typical Rate: \$45/m²

Precinct-wide initiatives

- Identify localised planting opportunities to start transforming utilitarian lanes into ‘mews streets’ and offer landowner planting grants for key locations
- Establish a tree valuation policy to attribute a financial value to large existing trees.
- Include planning incentives (additional height/Plot Ratio) for the retention of mature trees.



Introduce a pattern treatment to the road carriageways on Glyde and Stuart Streets and the section of Stirling Highway between Glyde and Stuart Streets.



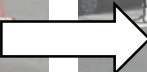
Plant street trees along the western verge of Stirling Highway where possible, perhaps with a Signature Tree – Cottesloe has Norfolk Pine, Peppermint Grove has Peppermints. Should Mosman Park have a signature tree and, if so, what might it be?



‘Book end’ the the precinct with landscape entry features on Stirling Highway to announce the arrival to the town centre precinct.



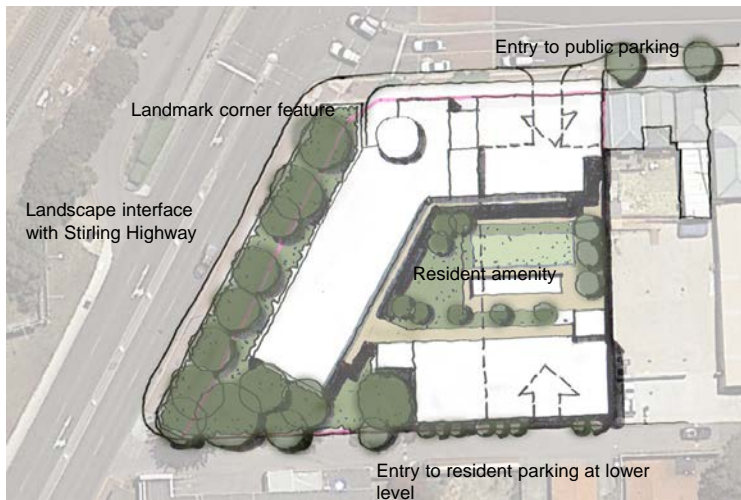
Plant dense landscaping along the western verge of Stirling Highway (and street trees on the eastern verge) to reduce the openness of the combined road/rail reserve



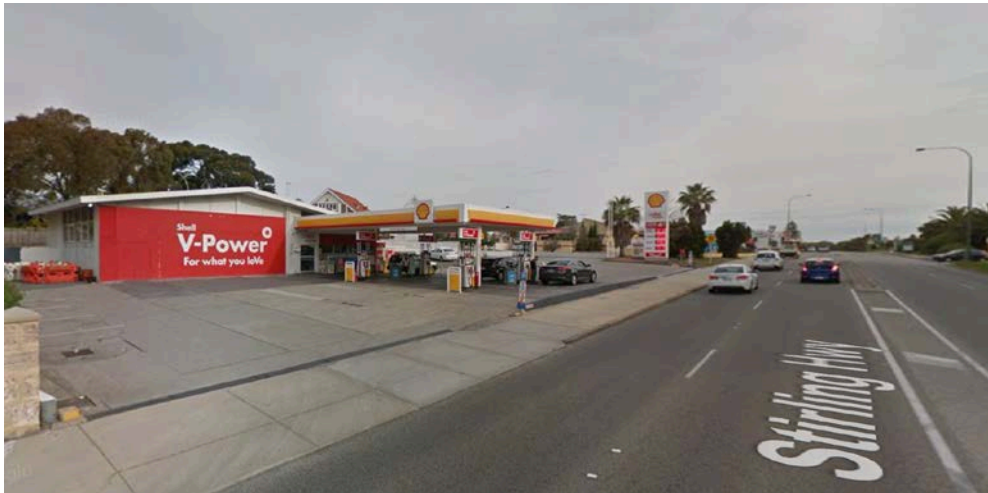
Plant trees in the IGA car park

Plant trees along the Glyde Street retail strip at the kerb line to improve pedestrian amenity and create a stronger sense of place.

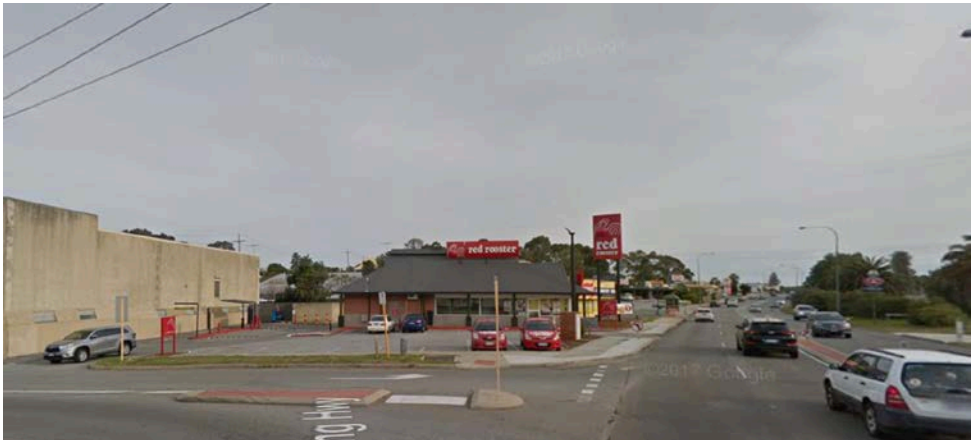




Incentivise the redevelopment of the apartment building on the southern side of Glyde Street at Stirling Highway to create a worthy landmark building.



Encourage/incentivise redevelopment of the Shell Garage for development that complements the St Luke's buildings (such as aged care/retirement living, etc)



Work with the Shire of Peppermint Grove for a better presence at the Johnstone Street entry to Mosman Park.



Establish a laneway vegetation program and establish modest setbacks for all development on laneways

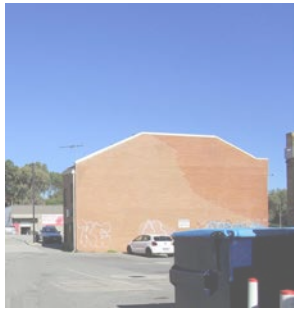
- ***What do you think of the landscape vision and palettes ie:***
 - ***A mix of species***
 - ***A mix of colours***
 - ***Landscaping at various levels (ground cover, shrubs and trees)***
- ***Do you have any other suggestions for a landscape palette eg. uniformity of species***
- ***Should Mosman Park have a signature tree and where should it be?***
- ***Do you agree with the concept of having a hierarchy of treatments for public infrastructure in the street and verge depending on location and use within the TCA?***
- ***Do you agree with using built form and landscaping to create an entry statement on Stirling Highway?***
- ***Do you have any comments on the landscape and streetscape initiatives?***

Place-making 'quick win' initiatives: Art walk



Re-envisage the north-south lanes (Ferrier and Haydock Lanes) as an 'art trail' of street art, sculptures and murals – extend through to St Luke's and the Gill St Café – to create an Art and Garden walking loop

Place-making 'quick win' initiatives: Urban art



Identify blank walls with opportunities to accommodate a new street art program

Include the painting of utility infrastructure in a street arts program

Place-making 'quick win' initiatives

'Quick wins' precinct-wide initiatives

- Re-envisage Mosman Park as 'The Gardens' - a habitable botanical garden – utilising verges, front setbacks and balconies, to establish a point of difference from Cottesloe, Claremont and North Fremantle.
- Establish a precinct-specific annual front yard gardening award.
- Establish a precinct-specific apartment balcony gardening award.
- Establish design guidelines for built form character.
- Establish a street-art program.
- Identify key blank walls as mural opportunities.
- Replace the street furniture to a common design theme.
- Establish 'gold, silver and bronze' palettes for street furniture and paving

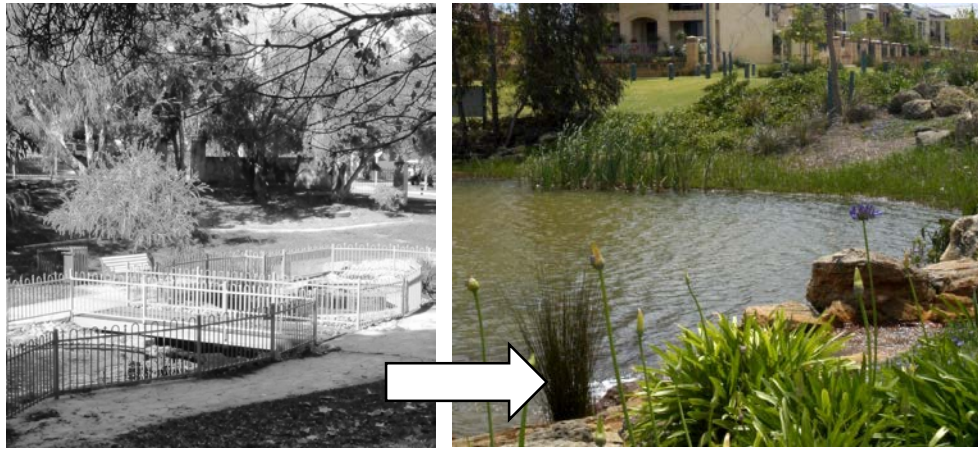


Plant trees in the triangles formed in the angled parking along Stuart Street.

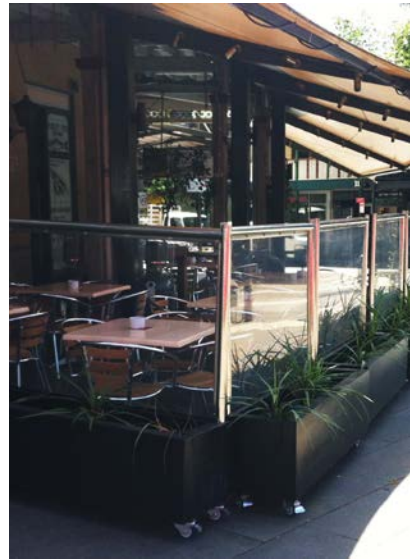


Introduce parking bay 'parklets' on the southern side of Glyde Street to increase the potential for al fresco activity with a northern aspect.

Place-making 'quick wins' initiatives



Reshape the Centenary Park pond to avoid the need for railings



Establish barriers at the front of Rodney's Bait and Tackle and other shops to enable some al fresco activity



Establish a regular 'picnic in the park' in Centenary Park with food trucks and food stalls around the park



Support business along Stirling Highway to activate the rear carpark.

Place-making 'quick wins' initiatives



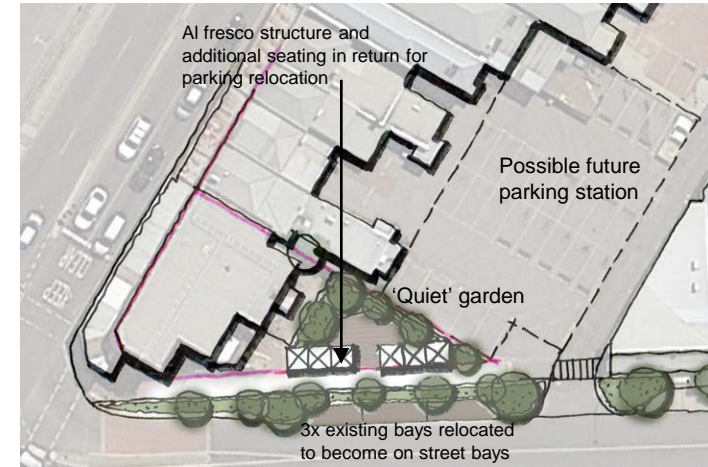
Introduce a cobbled strip adjacent to the kerbs on the Glyde Street retail core to 'soften' the transition from road to footpath.



Install festive lighting (bud and flood lighting) in new and existing trees in the town centre streets (Glyde, Stuart and Ecclesborne).



Paint the railings in Centenary Park black to make them less noticeable and less 'municipal'.



Create a pocket park on the north side of Glyde Street behind the corner building to Stirling Highway

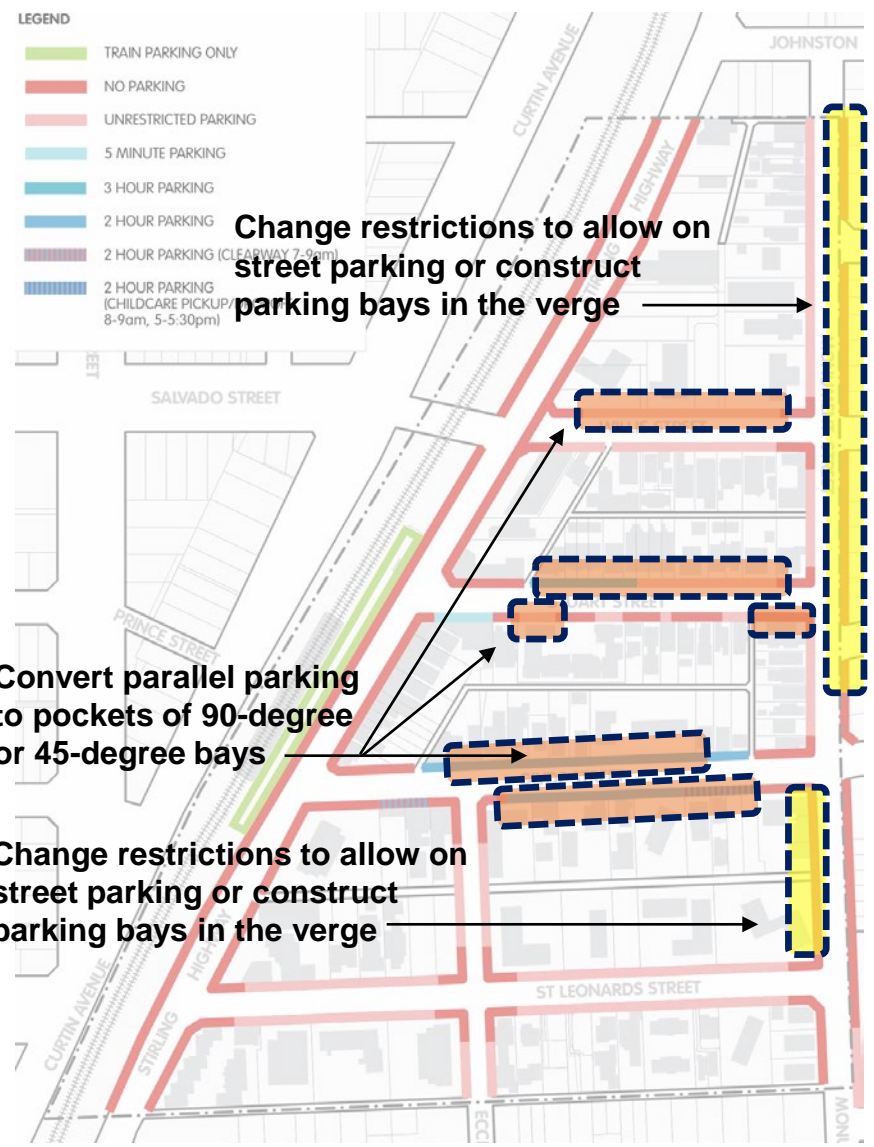
Place-making 'quick wins' initiatives

walking, biking and parking

Methods of transport other than private vehicles are to be provided for through:

- Creating a more pedestrian friendly environment to encourage walking through trees and awnings to provide shade
- Improving the pedestrian environment along Stirling Highway that provides access to the train station
- Encouraging bike parking both in private development and installing bike racks throughout the area

More parking can be made available through better utilisation of road space and minor modifications to verges if necessary. As redevelopment occurs, more private parking will be provided, helping to reduce demand for on-street parking.

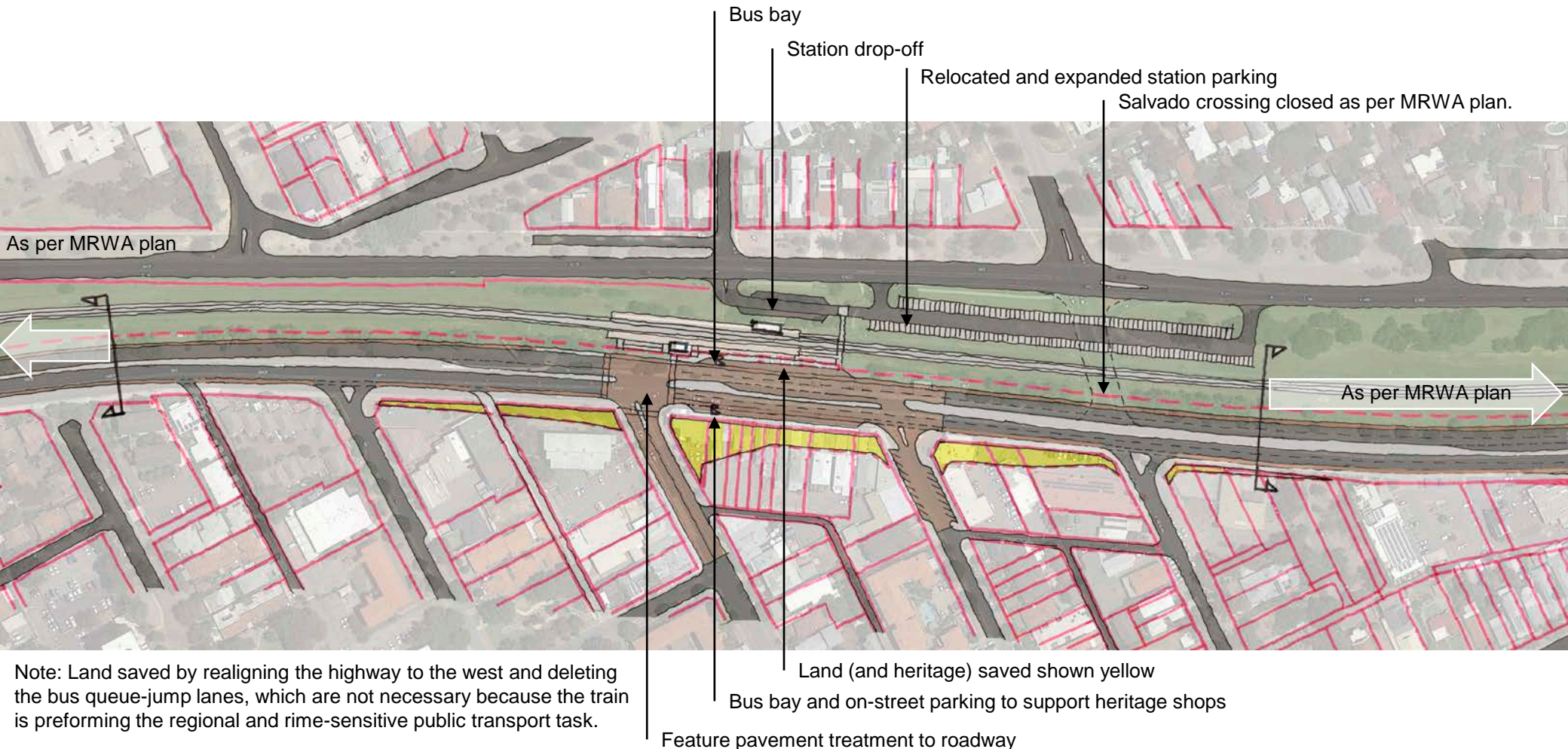




Identify locations for public parking stations funded from cash-in-lieu or developer contributions from new development in the Mosman Park town centre.

Place-making long term initiatives: Stirling Highway realignment

Re-align Stirling Highway further to the west, removing the park and ride but enabling the retention of the heritage shops and enabling on-street parking in front of them.



Note: Land saved by realigning the highway to the west and deleting the bus queue-jump lanes, which are not necessary because the train is performing the regional and time-sensitive public transport task.

- *What do you think of the ‘quick win’ initiatives?*
- *Are there any that have not yet been identified?*
- *Which ‘quick win’ initiatives would you like to see prioritised?*
- *What do you think about revisiting the alignment of Stirling Highway?*

A character statement and urban design principles have been suggested for the town centre area as a whole has a character statement and urban design principles.

Furthermore, bespoke character statements and an outline of design guidelines have been developed for the five precincts. Each precinct will have its own character statement and a set of design guidelines that outline design principles, street interfaces, built forms and materials and colours. The five precincts are:

- **Town Centre Core**
- **Highway**
- **Village**
- **Garden**
- **Church**

The final character statements and design guidelines will be incorporated into Local Development Plans for each area, which then become a statutory part of the planning framework to which new development is required to conform.



Vision statement for the town centre

Development in an urban village where architecture, landscape, and a range of different activities blend together to create a place that is busy, visually appealing, of a comfortable urban scale, remembers its past, and is distinguishable from its suburban surrounds.

General urban design principles for the town centre precinct

- Integrate the building type, land use, landscape, modes of transport and public space within the context of the site.
- Develop new built form to have an empathy with the local character and heritage.
- Create amenity for building users and people in the public realm.
- Ensure a high quality built form, aesthetic and landscape design that relates to the sense of place.
- Facilitate and support human activity.
- Incorporate measures to enhance safety and security.
- Articulate the built form to convey visual interest along the streetscape through design elements and richness of detail.
- Create robust and adaptable places that respond to the changing needs in the community and enhance the environment.
- Encourage design that is resource efficient, ecologically sustainable and climate responsive.
- Develop built form that integrates and supports the safe movement of people
- Deliver diverse new housing and adapted existing housing for different household sizes, demographics and affordability.

Character statement

A main-street precinct with an urban scale and a traditional character that is an evolution in scale from its past. The streets present a comfortable and appealing pedestrian realm with strong interaction between the ground floor uses and the street, whilst the upper floors provide for a range of residential and commercial uses. New architecture is a contemporary and larger-scaled interpretation of the existing traditional architecture, that follows the rhythm of the existing small narrow fronted shops at ground level and incorporates a palette of colours and materials drawn from the existing place.

Specific principles for the town centre core precinct

- Built form height shall respect the public realm and neighbouring sites, and include measures to lessen the impact of bulk and scale.
- Development shall respond to the context, site features and the ground level.
- Design shall respect the amenity of neighbours, such as minimising overshadow and overlooking.
- Buildings shall include opportunities for an interactive ground level next to the public realm.
- Architectural language shall continue the traditional design, details and social character of the town centre.



Building setback

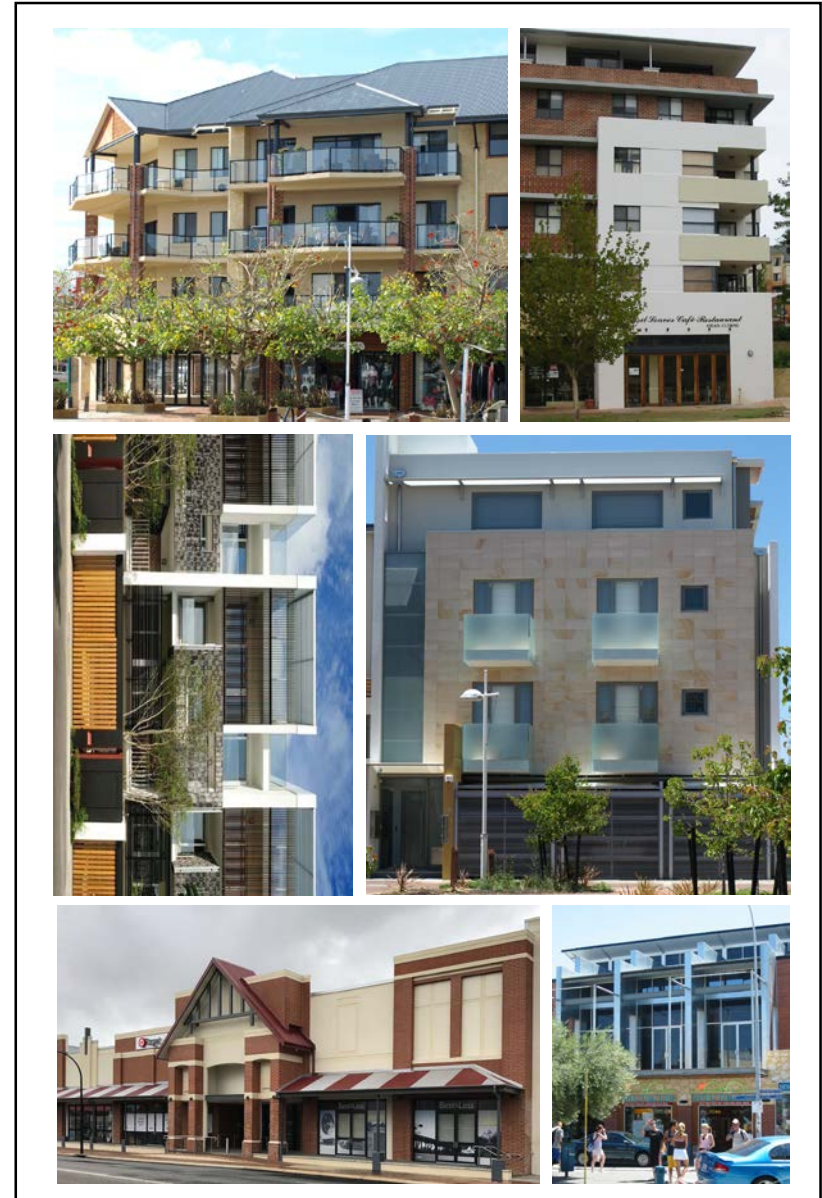
- A building shall have a nil front setback from the street boundary.
- At ground floor level, a building shall be built to both side boundaries to strengthen the cohesiveness of the Main Street streetscape and the continuity of the overhead pedestrian shelter. An exception is where a Pedestrian Access Way is desirable to connect a rear public space (such as a carpark) to the street frontage.
- For buildings above three storeys, the upper level shall be recessed to minimise the building bulk as seen from the footpath level of the pedestrian.

Built form and design aesthetic

- The building form shall be regular shaped and, for a large development, divided vertically into a clearly defined rhythm of smaller units with a typical shop front width of 6m to 10m.
- Openings shall be ordered with equal spacing and be vertically proportioned.
- The roof shall be either pitched with a deep overhang or concealed behind a parapet that has fine detailing.

Ground floor frontage

- A interactive frontage for commercial or retail use shall create opportunities for the function of the interior and the building's occupants to engage with the community in the street.
- Glazing shall be a minimum head height of 2.7m. Fixed openings shall include a raised sill of minimum 450mm above the floor level, or glazing shall be recessed within large 'punctured' openings. The sill may be lower where the glazed frontage opens to the street, such as bifold doors. All glazed openings shall include frames with fine, lightweight detailing.



- A main entry door shall be inset to create a welcoming experience.

Weather protection for pedestrians

- Pedestrian shelter shall be provided by a continuous and opaque element, such as a canopy or arcade, with the edge aligned with the curb of the footpath.

Signage

- Signage shall integrate with the design of the canopy over the footpath or with the shop front at the ground level.

Materials and colours

- The frontage of the building shall include at least two of the following materials in a compatible format: painted textured render, weatherboard, face brick, stone or ceramic tiles.
- Wall colours shall be warm and generally paler colours, with finer details in a contrast or complementary colour.

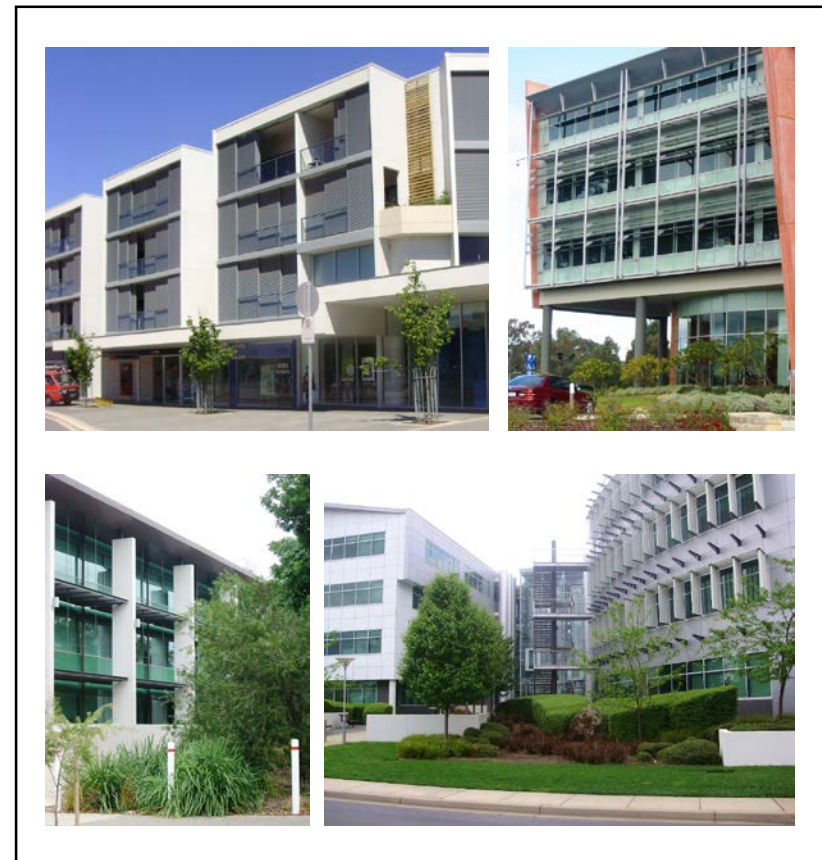


Character statement

A highly-visible commercial strip that is a memorable event in the journey along Stirling Highway. New buildings are contemporary, simple and elegant with a high-degree of transparency, with parking areas that include an abundance of shade trees. The green character is further enhanced by an elegant avenue of street trees that provide shade to the footpath, along with an extensively landscaped curtain to the town on the western side of the highway. The overall effect is of a 'parkway' rather than a 'highway'.

Specific principles for the highway commercial precinct

- Built form shall provide a well-defined edge to the public realm and modulate heights and setbacks at the interface with neighbouring precincts to lessen the impact of bulk and scale and minimise overshadowing and overlooking.
- Development shall establish an interactive commercial 'shopfront' at ground level with a high proportion of clear glazing.
- Built form shall be set back from the street boundary to allow for soft landscape beds and deep soil zones for large canopy trees and other vegetation.
- Built form shall be simple, contemporary and regular in proportion.
- Built form shall address Stirling Highway with a minimum of two levels.
- Upper building levels shall include opportunities for passive surveillance of the public realm.
- Driveway crossovers shall be minimised and shared between lots utilising easements and existing rights-of-way wherever possible.
- Carparking shall be screened behind, within, under or above buildings and any interim open car parks shall be landscaped with trees.

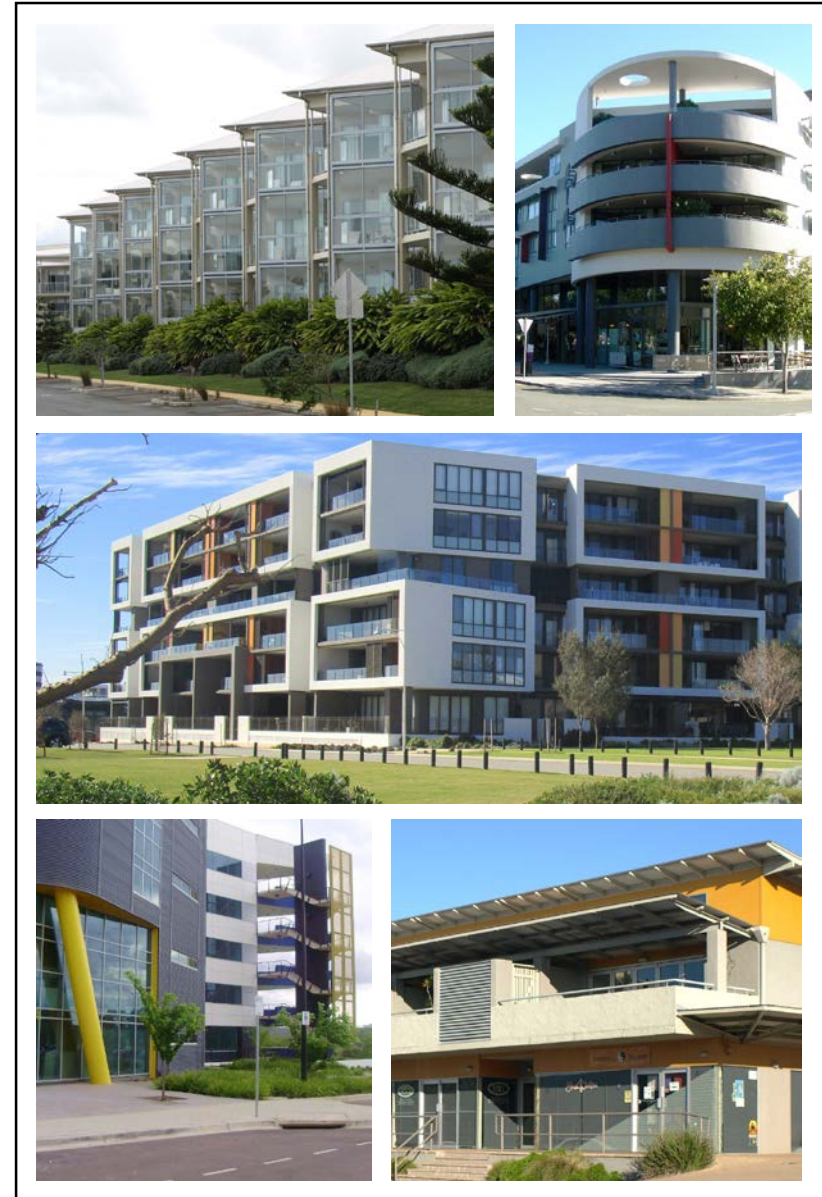


Setback and street boundary

- Building shall be setback from the front boundary to allow for a long length and wide width of soft landscape and large shade trees to create an attractive 'green Parkway edge' and amenity for the streetscape.
- The front shall be an open aspect to the street without fences.

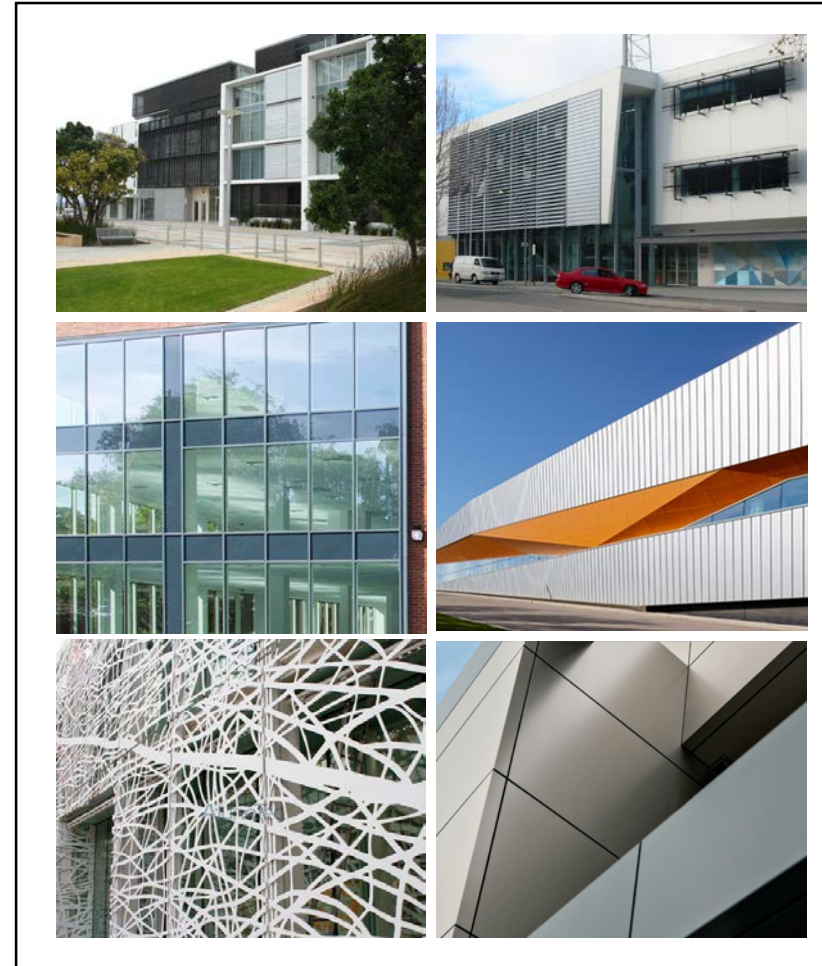
Built form

- The building aesthetic shall be bold and visually discernible from the Highway, modern and elegantly formed with the storey heights expressed horizontally for delineation of proportion.
- Roof form should be flat, shallow pitched, or if a mixed-use building, the roof may be creatively designed to express the residential use.
- The ground floor of the building shall align with the front boundary.
- The ground floor shall be largely glazed and interactive to represent a commercial business front.
- A building shall be two storeys minimum in a strongly expressed podium form, with any further upper levels being modulated to respond to and enhance the context, for example with setbacks.
- An upper floor shall include openings to enable passive surveillance of the public realm by building occupants.
- Openings should include chunkier commercial grade frames.
- Signage shall be Integrated attractively on the building.



Materials and colours

- Colours shall be bolder and in large expanses.
- Materials shall express a modern aesthetic, such as through the predominant use of metal, painted and through-colour cladding panels, glazing and portions of brick and timber.



Built form character: 'Village residential precinct'

Character statement

The 'Village' is an area where the past is harmoniously intertwined with the present, characterised by residential buildings that range from charming older single storey cottages to newer and taller height apartment buildings. Also in the mix is a range of commercial uses at ground floor level that complement, rather than compete with, the town centre. Two things hold the place together – continuity of well-landscaped street and front gardens, and an architecture that is inspired by the traditional architecture of the place through a 'thread' of colours, materials, proportions and roof forms from previous eras.

Specific principles for the village residential precinct

- Built form shall be setback from the street boundary to respect the character of the streetscape and to create a front garden with soft landscape.
- A garage or carport shall be setback behind the building frontage.
- Architecture shall be developed with a traditional design language.
- Building elevations that face the public realm shall be designed with openings for opportunities for passive surveillance and to enhance community safety and security.
- Design of the fence on the street boundary shall respect the character of the streetscape and provide opportunities for passive surveillance from within the lot.

Setback and street boundary

- The building's front setback area shall include attractive soft landscape and green canopy cover to increase the quantity in the area.
- A fence shall be low height and in timber picket or masonry with rails and should include a landscaped bed in front. Alternative fencing may includes metal rails between masonry piers.



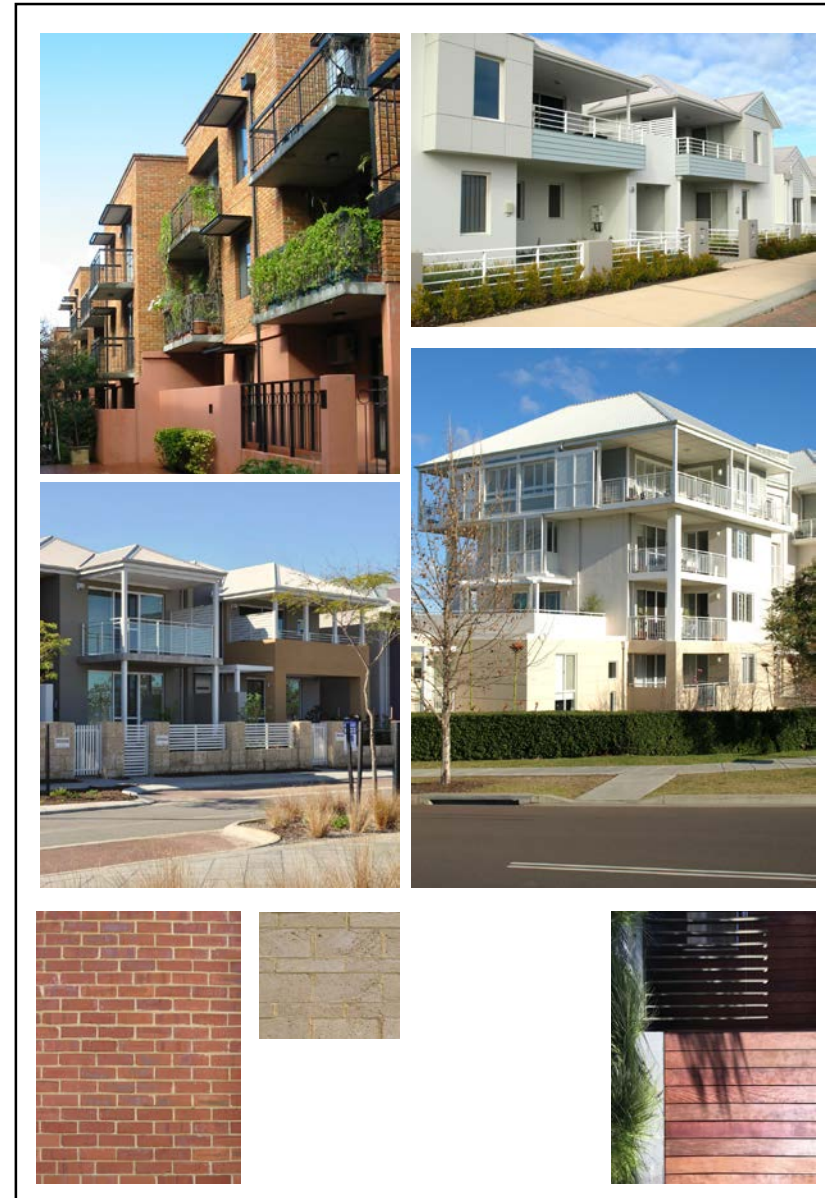
Built form character – Village residential precinct

Built form

- Built form shall include the use of a roof that is steeply pitched and may include gable ends.
- The building frontage shall be enlivened by including useable outdoor spaces of either a substantial veranda or balcony, together with small scale elements for visual richness, such as decorative fretwork, screens, mouldings and struts.
- The roof space should be used as a habitable room with a dormer window.
- A garage located and accessible from a rear lane shall include a prominent gable end with timber details.

Materials and colours

- The frontage of the building shall include at least two of the following materials in a compatible format: painted textured render, weatherboard, face brick or stone.
- A brick wall shall be in a cream, buff or red colour.
- Details such as railings, arches and canopies should be highlighted simply in black or white.
- The roof shall be either metal in a light colour or tiles in a red or brown colour.



Character statement

The 'Garden' area is an evolution of the existing character that is largely defined by modestly-scaled apartment building sitting in a well-landscaped setting evoking the aspirations of 20th century modernist planning and architecture. Regardless of size, new, predominately residential, development is characterised by well-planted and generous street setbacks and an architecture that continues the theme of simple building forms, bold horizontal elements, and quirky architectural details that were the hallmark of modern architecture.

At the rear, the existing laneways are transformed into mews streets with back fences and car parks replaced with modestly-scaled buildings and punctuated by incidental landscaping that helps to soften the streetscape of the laneways.

Specific principles for the garden residential precinct

- Primary street frontages shall be set back from the street boundary to allow for soft landscape and deep soil zones for large canopy trees and other vegetation character consistent with the existing streetscape.
- Site planning shall include open space around the building for landscape.
- Built form shall incorporate design elements to evoke a Modern era style of architecture.
- Car parking shall be screened from view of the primary street and any Mews Street (rear lane).
- New development shall incorporate climate responsive design.
- Where utilised, fence design shall include opportunities for passive surveillance of the public realm.



Building setback and boundary

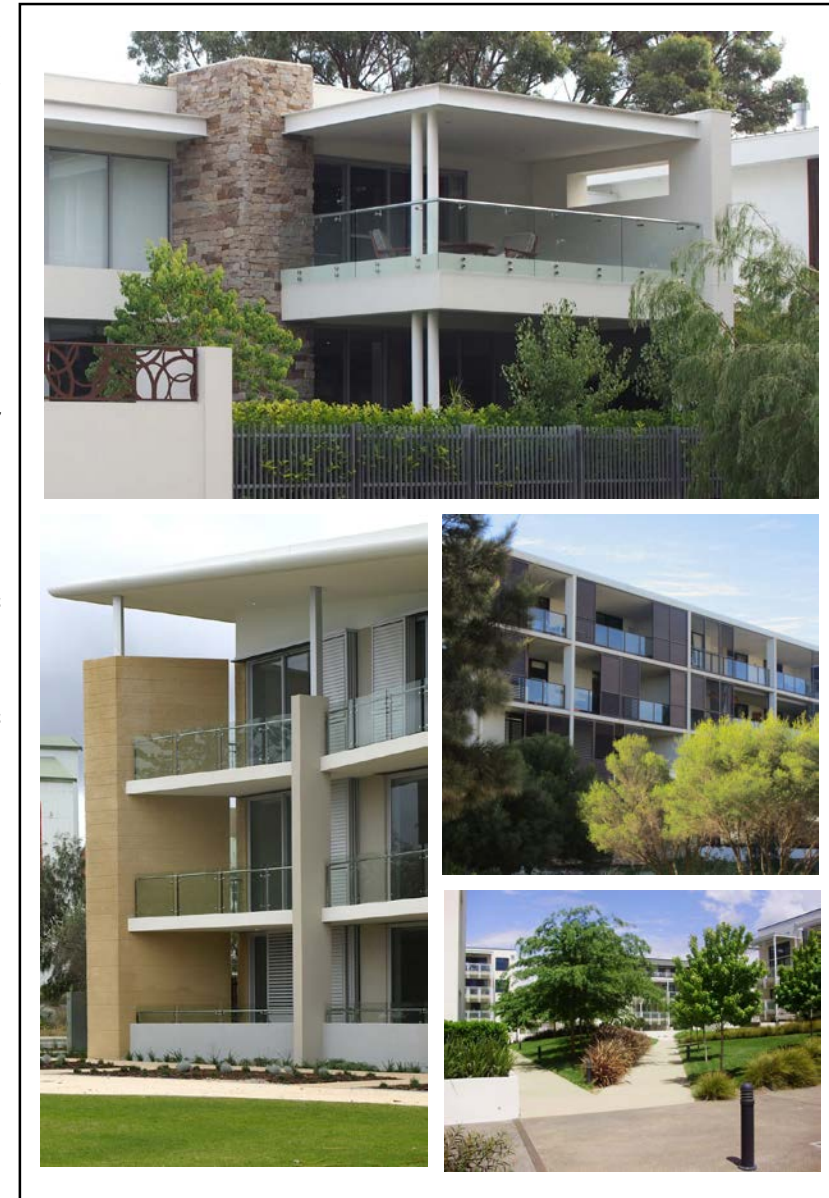
- The building shall be setback from the street boundary and recessed from the side boundaries to create a detached building and to allow for an expansive area of soft landscape and green canopy cover to be visible from the public realm.
- A front fence shall include a masonry base with railings above for visual permeability.

Built form

- Building design should adopt the use of structural innovation.
- The building frontage shall include an emphasis on rectangular and linear elements within the storey, including banding in white.
- Openings shall be ordered and either grouped or paired together.
- The building aesthetic shall express elegance and an uncluttered simplicity of design and avoid applied decoration.
- The building shall respond to climate through, for example, the use of breezeways and shade structures.

Materials and colours

- The design of the structure and use of materials should be rational and 'honest' to be consistent with the architectural principles of Modernist architecture, such as load bearing brickwork or frame construction with cladding.
- Masonry shall be in buff, brown or another warm light colour.



Built form character: Church precinct

Character statement

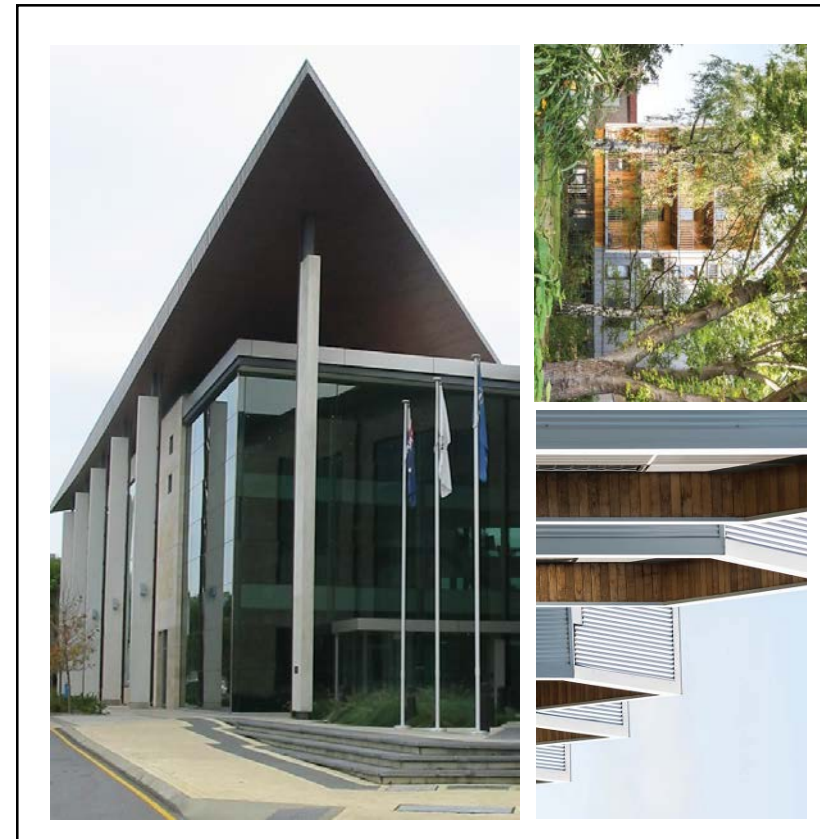
The Church precinct is a small enclave distinguished by a collection of ecclesiastical buildings. New development in the Church precinct respects the existing ecclesiastical buildings by maintaining generous landscaped spaces between new and old, utilising a strong and simple pitched roof along with a predominance of brick and timber detailing in the elevations. The architecture of new buildings in the Church precinct is generally more ‘formal’ with strong geometric forms, symmetry, repetition and axial vistas, as opposed to organic forms or buildings composed of a collage of elements.

Specific principles (unique principles applied to the church sub-precinct)

- New development shall conserve and maintain the existing built form and landscape where possible.
- Site planning shall respond to a regular layout and a formal composition of buildings and landscape.
- New built form shall include its own curtilage of open space to continue the notion of a family of independent buildings
- New architecture and landscape elements shall respect the heritage language of geometrical and symmetrical form, fine details, and the distinctive palette of materials and colours of the existing buildings.

Building setback and boundary

- A building shall be detached and set in a formal, open and attractive landscape for the three-dimensional form to be visible from the public realm.
- Building shall be aligned with the front boundary.
- The fence shall be an open railing style to allow a view of the the building in its landscaped setting.



Built form

- The building shall express a simple solid, volumetric and regular shaped form.
- Architectural elements shall be repeated on the front elevation at different scales, such as a gable end with a matching porch form and clearly visible front door.
- Openings shall be vertically proportioned and narrow 'punched' openings in the wall.
- Visual richness shall be included either as part of the construction, such as an arch with a brick surround or bands, or an applied detail such as timber fretwork.
- The roof form shall include a steep pitch.

Materials and colours

- Building wall materials shall be selected from masonry, limestone, painted render and small areas of timberwork.
- Wall colours shall be light or red if brick.
- The roof shall be either light coloured metal or red or brown tiles.



- ***Considering the character statement and design guidelines for each precinct:***
 - ***What do you think of the preliminary design guidelines?***
 - ***Do they capture an appropriate response to each of the precincts?***
 - ***Is there anything else about the character of the buildings that you think the design guidelines should cover?***
- ***What, if anything would you change for the:***
 - ***Town Centre Core Precinct***
 - ***Highway Precinct***
 - ***Village Precinct***
 - ***Garden Precinct***
 - ***Church Precinct***

Where to from here?

