



# TOWN OF MOSMAN PARK

## LOCAL PLANNING POLICY

<b>POLICY NAME:</b>	<b>MOSMAN PARK TOWN CENTRE PLANNING AREA</b>
<b>POLICY NO.</b>	<b>LPP 20</b>
<b>OLD POLICY NO:</b>	
<b>ADOPTION DATE:</b>	<b>ORDINARY COUNCIL MEETING –</b>
<b>EFFECTIVE FROM:</b>	

### 1. POLICY STATEMENT

To support implementation of the Mosman Park Town Centre Plan and provide design guidance for proposed development within the Mosman Park Town Centre Area in accordance with the Community Vision for the Town Centre area.

### 2. RELATED LEGISLATION AND STATUTORY REQUIREMENTS

- *Planning and Development (Local Planning Schemes) Regulations 2015* (the Regulations)
- Town of Mosman Park Local Planning Scheme No. 3 (LPS3, The Scheme)
- State Planning Policies:
  - SPP 7.3 – Residential Design Codes Volumes 1 and 2
- Local Planning Policies:
  - LPP04 – Subdivision and Development of Lots Adjoining Laneways
  - LPP14 – Building Height and Natural Ground Level
  - LPP15 – Development Standards for Multiple Dwellings, Mixed-Use Developments and Non-Residential Developments
  - Other Local Planning Policies.

This Local Planning Policy has been prepared under the provisions of Schedule 2, Part 2, Clauses 3 and 4 of the Regulations.

#### 2.1 Relationship with the Planning Framework

This Policy forms part of the Town's planning framework and should be read in conjunction with the Scheme, the R-Codes, and other Local Planning Policies. Further to this:

- a) LPS3 prevails over this Policy to the extent of any inconsistency.

- b) This Local Planning Policy (LPP) prevails over any other LPP, including LPP15 to the extent of any inconsistency between LPP's.
- c) An approved Local Development Plan (LDP) or LPP which provides more specific development standards to development within a particular area prevails over this Policy to the extent of any inconsistency.

### 3. DEFINITIONS

Apart from the terms listed below, words and expressions used in this Policy have the same meaning as they have in the Regulations, LPS3, the R-Codes or other relevant legislation.

<b>Active land uses</b>	Means a shop, restaurant, small bar, café, or other similar land uses that promote streetscape interaction and vibrancy.
<b>Apartment</b>	A dwelling: <ul style="list-style-type: none"> <li>- in a building containing two or more dwellings; or</li> <li>- in a mixed use development</li> <li>- but does not include a grouped dwelling.</li> </ul>
<b>Communal open space</b>	Outdoor areas within the lot at ground level or on a structure that is accessible to and shared by residents for common recreational use and in some instances accessible to the public that promotes gathering and social interaction. It does not include primary external circulation areas for vehicles or pedestrians but includes a seating niche or small gathering space within a circulation space.
<b>Deep soil area</b>	Soft landscape area on the lot with no impeding building structure or feature above or below, which supports growth of medium to large canopy trees and meets a stated minimum dimension. Deep soil areas exclude basement car parks, services, swimming pools, tennis courts and impervious surfaces including car parks, driveways and roof areas.
<b>Mixed-use development</b>	A development that contains residential and non-residential uses, either in the same building or in separate buildings on the same lot.
<b>Non-residential development</b>	Any development that is not controlled by the R-Codes and includes, but is not limited to, wholly commercial development.
<b>Public open space</b>	Publicly accessible land set aside for the purpose of public enjoyment and protection of unique, environmental, social and cultural values for existing and future generations. It is vested in or under the control of a public authority.
<b>Roof garden</b>	An outdoor area on the roof of a development comprising no less than 50% of the total roof area, where the area of hardscape and impermeable covered area comprises a maximum of 40% of the

	overall roof garden area with the balance to comprise permanent natural vegetation. For the purposes of measuring building height, a roof garden shall not be counted as a storey of a building where these requirements are satisfied.
<b>Significant tree</b>	A tree with a minimum height of 4m and a trunk circumference of 500mm one metre above ground level.
<b>Soft landscape</b>	Any landscaped area with a minimum soil depth of 300mm that contains in-ground planting, and excludes removable planter boxes/pots and porous paving areas but includes turf.
<b>Streetscape</b>	The visible components in a street between the facing buildings, including the form of the buildings, garages, setbacks, fencing, driveways, utility services, street surfaces, street trees and street furniture such as lighting, signs, barriers and bus shelters.
<b>Zone boundary</b>	Means the boundary where two or more pieces of land have a different zone and/or are subject to a different R-Code. Includes land separated by a laneway but excludes land separated by a street.

#### 4. APPLICATION

- a) This Policy applies throughout the Mosman Park Town Centre Area being that bounded by Stirling Highway, the Shire of Peppermint Grove boundary, Monument Street and Laing Lane.
- b) Unless otherwise specified, this Policy applies to all development including single houses, grouped dwellings, multiple dwellings, non-residential developments and mixed-use developments.
- c) Deemed-to-Comply provisions and Design Principles apply to those forms of development subject to the R-Codes Volume 1 (single houses and grouped dwellings).
- d) Acceptable Outcomes and Element Objectives apply to development subject to the R-Codes Volume 2 (multiple dwellings) and non-residential developments and non-residential components of mixed-use developments.

## **5. OVERVIEW**

### **5.1 Mosman Park Town Centre Plan**

The Mosman Park Town Centre Plan is a spatial, Policy and Action Plan intended to build upon the inherent characteristics of the Town Centre area to deliver the Community Vision for the Town Centre. The Town Centre Plan comprises a number of foundation strategies which will collaborate to deliver the vision for the Town Centre.

These address Policy requirements to promote future development that reflects the built form objectives of the Community Vision, a movement strategy to design and then deliver efficient movement to and through the Town Centre, a street and land scape strategy that will deliver the amenity objectives of the plan and a Place Making and Activation Strategy.

### **5.2 Town Centre Built Form – Local Planning Policy No. 20**

The built form strategy is established by this Policy (LPP 20) which will operate in collaboration with the wider Town Planning Framework, in particular the “Residential Design Codes” and “Local Planning Policy No. 15 – Development Standards for Multiple Dwellings, Mixed-Use Developments and Non-Residential Developments”, but contains additional guidelines and development guidance specific to the Town Centre area to achieve built form within the Town Centre that will reflect the objectives of the Mosman Park Town Centre Plan.

### **5.3 Mosman Park Town Centre Vision Statement**

Consultation with the community, resulted in the identification of a vision for the Mosman Park Town Centre;

“An urban village which blends architecture, landscape and activity to create a busy and visually appealing place that remembers its past and is distinctive from its surrounds.”

This Policy has been prepared in recognition of and to support this vision. Any development proposed within the Town Centre should seek to respond to the vision for the Town Centre.

### **5.4 Mosman Park Town Centre Generic Design Principles**

Generic Design Principles have been identified to support implementation of the vision for the Mosman Park Town Centre. These principles will apply across the whole Town Centre area and any development proposal within the Town Centre area should be designed within the context of these general principles:

- Ensure a high-quality built form, aesthetic and landscape design that relates to the sense of place.
- Incorporate botanical and landscape elements as an integral component in built form.
- Design new built form in a manner that has empathy with the local character and heritage of the Town centre by either:
  - Using traditional architectural forms and elements, or
  - Interpreting traditional architectural forms and elements, in a contemporary way.
- Deliver commercial uses, where appropriate, that contribute to a diversity of local services and increase the appeal of the Town Centre to visitors and locals alike.
- Create amenity for both the users of a new development and people in the public realm.
- Enable strong interactions between building occupants and people in the public realm and avoid presentation of blank walls to streets.
- Develop built form that integrates and supports the safe movement of people within the public realm.
- Articulate the built form to convey visual interest along the streetscape through design elements, richness and depth of detail.
- Create robust and adaptable places that respond to the changing needs in the community and enhance the environment.
- Design buildings that are resource efficient, ecologically sustainable and responsive to climate.
- Integrate vegetation into architectural design, using setback areas, courtyards, roof gardens and climbing vegetation.
- Deliver diverse new housing and adapted existing housing for different household sizes, demographics and affordability.

## 6. POLICY PROVISIONS

Not all elements of building design are provided for in this Policy. The comprehensive list of design elements below provides a guide as to which planning instrument contains the applicable provisions:

**TABLE 1: DESIGN ELEMENTS PROVISIONS**

DESIGN ELEMENT	LOCATION OF PROVISIONS	
	For Residential Development (and residential components of mixed-use development)	For Non-Residential Development (and non-residential components of mixed-use development)
<b>Plot Ratio</b>	R-Codes Element 2.5	LPP 15, Clause 5.1.1.1
<b>Building Composition</b>	N/A	LPP 15, Clause 5.1.1.2
<b>Building Height</b>	LPP 14, LPS 3, LPP 15, Clause 5.1.2.1	LPP 14, LPP 15, Clause 5.1.3.1
<b>Street Setback</b>	LPP 15, Clause 5.1.2.2, LPS 3 Table 6 (zone dependent) and this Policy Tables 2, 4, 6 and 8	LPP 15, Clause 5.1.3.1, LPS 3 Table 6 (zone dependent) and this Policy Tables 2, 4, 6 and 8
<b>Side Setbacks</b>	R Codes, Element 2.4, LPS 3 Table 6 (zone dependent) and this Policy Tables 2 and 8	LPP 15, Clause 5.1.3.2, LPS Table 6 (zone dependent) and This Policy Tables 2 and 8
<b>Building Depth</b>	R-Codes , Element 2.6	N/A
<b>Building Separation</b>	R-Codes, Element 2.7	This Policy Table 10
<b>Open Space</b>	N/A	LPP 15, Clause 5.1.3.4
<b>Tree Canopy and Deep Soil Areas</b>	R-Codes, Element 3.3	R-Codes, Element 3.3 and this Policy Tables 6 and 8
<b>Communal Open Space</b>	R-Codes, Element 3.4	N/A
<b>Public Domain Interface (including fences)</b>	R-Codes, Element 3.6, LPP 15, Clause 5.1.2.4 and this Policy Tables 2, 6, 8 and 10	LPP 15, Clauses 5.1.3.5 and 5.1.3.6, and this Policy, Tables 2, 6, 8 and 10
<b>Pedestrian Access and Entries</b>	R-Codes, Element 3.7	R-Codes, Element 3.7
<b>Solar and Daylight Access</b>	R-Codes, Element 4.1	N/A
<b>Natural Ventilation</b>	R-Codes, Element 4.2	N/A
<b>Circulation and Common Spaces</b>	R-Codes, Element 4.5	N/A
<b>Storage</b>	R-Codes, Element 4.6	N/A
<b>Managing Noise Impact</b>	R-Codes, Element 4.7	N/A
<b>Dwelling Mix</b>	R-Codes, Element 4.8	N/A
<b>Universal Design</b>	R-Codes, Element 4.9	N/A

<b>Roof Design</b>	R-Codes, Element 4.11 and this Policy Tables 8 and 10	This Policy, Tables 8 and 10
<b>Adaptive Reuse</b>	R-Codes, Element 4.13 and this Policy Table 2	This Policy, Table 2
<b>Mixed Use</b>	R-Codes, Element 4.14	R-Codes, Element 4.14
<b>Energy Efficiency</b>	R-Codes, Element 4.15	N/A
<b>Waste Management</b>	R-Codes, Element 4.17	LPP 15, Clause 5.1.3.10
<b>Façade Design</b>	R-Codes, Element 4.10 and this Policy Tables 2, 4, 6 and 10	LPP 15, Clause 5.1.3.5 and this Policy Tables 2, 4, 6 and 10
<b>Private Open Space and Balconies</b>	R-Codes, Element 4.4	N/A
<b>Landscape Design</b>	R-Codes, Element 4.12	R-Codes, Element 4.12 and this Policy Table 10
<b>Car and Bicycle Parking (including design)</b>	R-Codes, Element 3.9	LPS 3, Table 6, Clause 5, LPP 15, Clauses 5.1.3.7 and 5.1.3.8, and R Codes Element 3.9
<b>Vehicle Access</b>	R-Codes, Element 3.8 and LPS 3, Table 6, Clause 7	R-Codes, Element 3.8 and LPS 3, Table 6, Clause 7
<b>Water Management and Conservation</b>	R-Codes, Element 4.16	R-Codes, Element 4.16
<b>Visual Privacy</b>	R-Codes, Element 3.5	LPP, Clause 5.1.3.9
<b>Orientation</b>	R-Codes, Element 3.2 and this Policy Table 6	R-Codes, Element 3,2 and this Policy Table 6
<b>Size and Layout of Dwellings</b>	R-Codes, Element 4.3	N/A
<b>External Fixtures (and solar collectors)</b>	LPP 13	LPP 13
<b>Utilities</b>	R-Codes, Element 4.18	R-Codes, Element 4.18
<b>Reflective Roof Materials</b>	LPP 07	LPP 07
<b>Laneway Widening</b>	LPP 04	LPP 04
<b>Pergolas/Unroofed Structures</b>	LPP 02	LPP 02
<b>Tennis Court Lighting and Fences</b>	LPP 05	LPP 05

## 6.1 Requirements

This Policy contains:

- 1) Required built form standards in the form of Design Guidelines; and

- 2) Recommended Design Guidance for each of the five (5) precincts within the Town Centre Area.

The precinct boundaries are shown in Figure 1.



Figure 1: Town Centre Precincts

### 6.1.1 Design Guidelines

The Design Guidelines are compulsory requirements and must be addressed as part of any development proposal within the Town Centre area in accordance with the applicable Design Guidelines for the precinct within which the development is proposed.

Each Design Guide Line is accompanied by a statement of intent to clarify the objective/purpose the guide line is intended to support. The onus is on the Applicant to demonstrate that the proposal meets the compulsory Design Guidelines to the satisfaction of the Town on the advice of the Town's Design Review Panel.



## 6.1.2 Design Guidance

Statements of Design Guidance are not compulsory, however, it is recommended they are addressed as part of any development proposal. Any development proposal that seeks either a variation to a standard requirement or any form of development bonus must address the Design Guidance Statements as part of the application.

Each Design Guidance is accompanied by a statement of intent to clarify the objective/purpose the guidance is intended to support. The onus is on the Applicant to demonstrate that where applicable, the proposal achieves appropriate delivery of Design Guidance items to the satisfaction of the Town on the advice of the Town's Design Review Panel.

## 6.1.3 Key Sites

A number of key sites within the Town Centre area are also identified. Specific items to be addressed as part of any development proposal for these sites are noted in this Policy.

## 6.2 Mosman Downtown Precinct

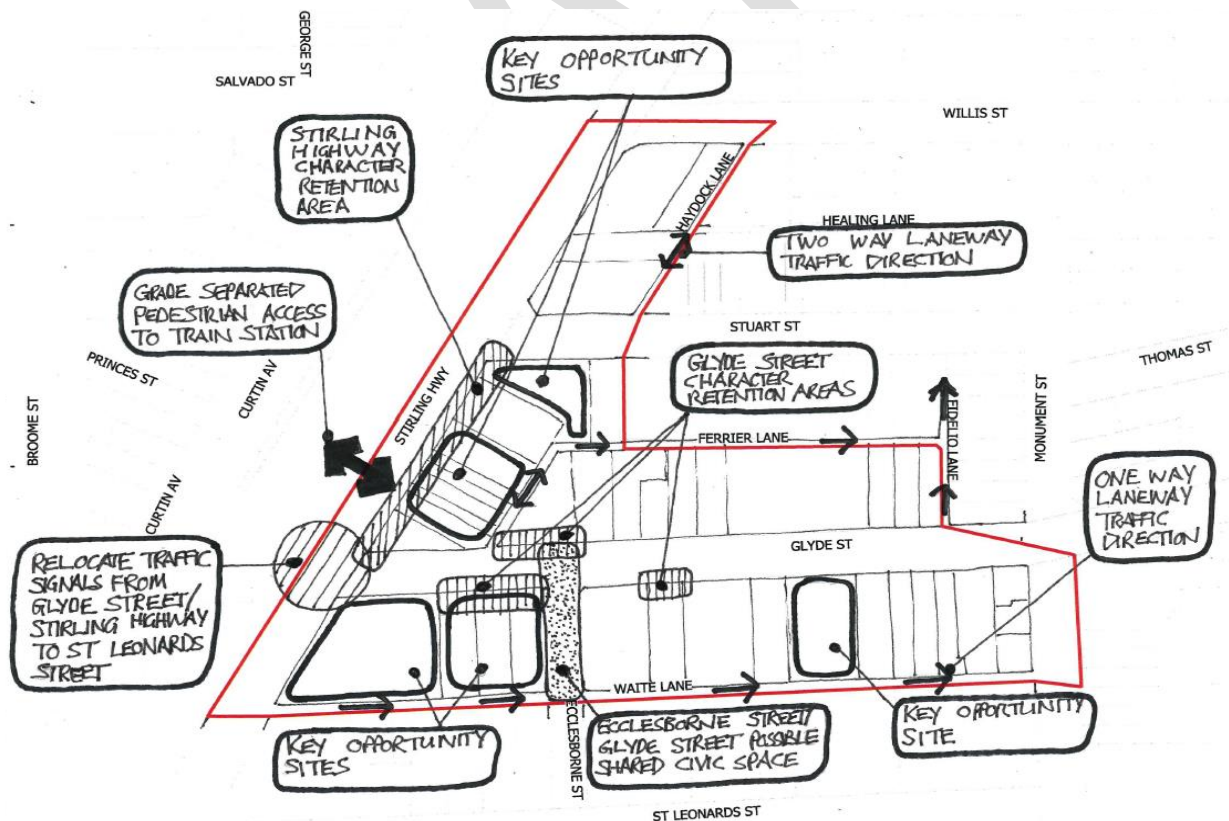


Figure 2: Mosman Downtown Precinct Plan

### 6.2.1 Mosman Downtown Precinct Character Statement

A main street precinct with an urban scale and a traditional character that is an evolution in scale from its past. The streets present a comfortable and appealing pedestrian realm with strong interaction between the ground floor uses and the street, whilst the upper floors provide for a range of residential and commercial uses.

New architecture is a contemporary and larger scaled interpretation of the existing traditional architecture:

- Which follows the rhythm of the existing smaller narrow fronted shops at ground level;
- That incorporates a palette of colours drawn from the existing place;
- Where built form and height respects the public realm and neighbouring sites by minimising bulk and scale, overshadowing and overlooking;
- Where development supports activation of laneways and the architectural language of the precinct continues the traditional design detail and character of the Town Centre; and
- Where retention of historic shop front facades maintains a link between the past and the future.

### 6.2.2 Mosman Downtown Precinct Design Guidelines

**TABLE 2: MOSMAN DOWNTOWN PRECINCT DESIGN GUIDELINES**

Design Guide Line		Guide Line Intent
DGL 6.2.2.1	A nil setback to all street boundaries other than laneways shall be provided unless a greater setback is considered appropriate at the discretion of the Council to secure a specific outcome considered desirable such as preservation of mature trees or provision of a public space.	To strengthen the cohesiveness of the streetscape.
DGL 6.2.2.2	Buildings shall be setback a minimum of 1m from a laneway boundary to be measured from the existing boundary or in the instance where widening is required, from the required widened laneway boundary.	To enable/increase provision of some landscaping in the laneway streetscape.
DGL 6.2.2.3	At ground floor level a building shall be built to both side boundaries along the street frontage with the exception where a pedestrian access way is considered desirable at Council's discretion to connect a rear public space or laneway to the street frontage and/or where necessary to accommodate a variation under DGL 6.2.2.1	To strengthen the cohesiveness of the main street streetscape and provide for continuity of overhead pedestrian shelter.

DGL 6.2.2.4	Above 3 storeys (4+ storeys) buildings shall be setback a minimum of 3m from all boundaries.	To minimise/reduce building bulk as seen from street level by creating a podium above 3 storeys
DGL 6.2.2.5	The ground floor of all development shall be set aside and designed and constructed to ultimately be capable of accommodating commercial/active use.	To ensure non-residential activity at ground level to support street activation and to provide for efficient conversion of less active commercial use to more active use over time.
DGL 6.2.2.6	The ground floor of all development shall be designed to include a glazed facade of a minimum of 70% of the façade area and a minimum head height of 2.7m.	To: <ul style="list-style-type: none"> <li>• Create opportunities for the function of the building interior and the building's occupants to engage with the public realm; and</li> <li>• To add interest and contribute to street level activation while reflecting historic shop front presentation in the Town Centre core.</li> </ul>
DGL 6.2.2.7	All development shall provide a ground floor awning/canopy element extending from the street boundary but which shall be no closer than 300mm to the road carriageway kerb, and which shall be no less than 2.7m above ground level.	To: <ul style="list-style-type: none"> <li>• Provide weather protection for pedestrians,</li> <li>• Assist in street level activation,</li> <li>• Reflect historic shopfront presentation, and</li> <li>• Establish a consistent streetscape identity for the Town Centre core.</li> </ul>
DGL 6.2.2.8	All signage proposed for the ground floor awning/canopy shall integrate with the design of the awning/canopy and shall not be less than 2.7m above ground level where suspended from the awning/canopy.	To ensure compatible signage that supports business but is consistent with a cohesive streetscape vision.

### 6.2.3 Mosman Downtown Precinct Design Guidance

**TABLE 3: MOSMAN DOWNTOWN PRECINCT DESIGN GUIDANCE**

Design Guidance		Guidance Intent
DG 6.2.3.1	Building form should be regular in shape. For development 3 storeys or greater in height, building form should be defined by vertical elements.	To reflect historic building form and promote cohesive precinct character.
DG 6.2.3.2	Ground floor retail space should reflect traditional shop front width within a range of 6 to 10m.	To reflect historic shop front characteristics inherent in the Town Centre area
DG 6.2.3.3	Openings in buildings should be ordered with equal spacing and be vertically proportioned.	To reflect historic building form and promote historic precinct character.
DG 6.2.3.4	Building roofs should be either pitched with a deep overhang, or alternatively, if flat should be concealed behind a parapet that includes fine detailing.	To reflect historic building form and promote historic precinct character.
DG 6.2.3.5	Ground floor entry doorways should be inset a minimum of 500mm.	To reflect historic building form and promote historic precinct character.
DG 6.2.3.6	The primary street façade of any building should include at least two (2) of the following materials in a compatible format: <ul style="list-style-type: none"> <li>• Painted textured render;</li> <li>• Weatherboard;</li> <li>• Face brick;</li> <li>• Stone, or</li> <li>• Ceramic tiles.</li> </ul>	To reflect historic Town Centre building characteristics and promote cohesive precinct character.

### 6.2.4 Mosman Downtown Key Opportunity Sites

There are a number of key opportunity sites within the Downtown Precinct which, as a result of their specific characteristics provide an opportunity to achieve additional outcomes in support of the vision for the Mosman Park Town Centre area.

Items specific to each of these sites are required to be considered as part of any development proposed. Where these matters are addressed to the satisfaction of the Town as part of any development proposal, the proposal will be considered to have satisfactorily met the “Development Incentive 11” requirements of Part D of Local Planning Policy 15 (LPP 15) enabling consideration of possible development bonuses in accordance with LPP 15.

#### 6.2.4.1 No. 630 (Lot 50) Stirling Highway

630 Stirling Highway is a large site (2906m<sup>2</sup>) strategically located at the corner of Stirling Highway and Glyde Street and with frontage to Waite Lane. The following matters should be considered as part of any proposed redevelopment of the site:

- Provision of an inherent landmark/iconic design element at the corner of Stirling Highway and Glyde Street as part of the building design,
- Retention/protection of existing mature trees adjacent to the Waite Lane boundary as a transition to the St Leonards Green precinct and to contribute to improved land and street scaping of Waite Lane,
- Provision of a pedestrian linkage along Waite Lane in conjunction with retention of the mature trees,
- Investigate opportunity to provide/incorporate public car parking accessed off Glyde Street as part of redevelopment of the site.

#### 6.2.4.2 No. 8 (Lot 4), 10 (Lot 5) and 16 (Lot 6) Glyde Street

The site comprises three (3) adjoining properties which combined create a large development site (2276m<sup>2</sup>). The combined site has frontage to Glyde Street, Ecclesborne Street and Waite Lane and abuts the 630 Stirling Highway key opportunity site. The site contains existing shops identified as part of the Glyde Street Character Retention Area and overlooks Centenary Park to the south.

The following matters should be considered as part of any proposed redevelopment of the site:

- Retention of the historic strip shops as per Clause 7.6 following,
- Inherent design measures to promote continuation of commercial land use activities as part of Ecclesborne Street ground level frontage,
- Provision of an additional ground level setback along Waite Lane frontage to facilitate ground floor activation overlooking Centenary Park, landscaping and pedestrian access along Waite Lane,
- Inclusion of design measures to ensure that the shadow cast at midday on 21 June unto Centenary Park does not exceed 50% of the Park area.

#### 6.2.4.3 Nos. 610 to 620 (Lots 27, 9,10 19, 20 and 11) Stirling Highway

The site comprises six (6) adjoining properties which combined create a large development site (2178m<sup>2</sup>). The site has frontage to Stirling Highway, Glyde Street and Ferrier Lane but is currently subject to a road widening requirement along the Stirling Highway frontage which would significantly reduce its area. It contains existing shops identified as part of the Stirling Highway Character Retention Area which would be demolished if the existing road widening is enacted.

The Mosman Park Town Centre Plan proposes an alternative approach that would avoid the need for road widening at this location if adopted. In the instance that this alternative is adopted and road widening not required, the following matters should be considered as part of any redevelopment of the site:

- Retention of the historic strip shops as per Clause 7.6 following,
- Consideration of a grade separated pedestrian link across Stirling Highway to the Mosman Park train station as an inherent part of the proposed development design, and
- Opportunities to incorporate public car parking accessed from Ferrier Lane as part of any redevelopment proposal.

In the event that road widening is enforced, then the following matters should be considered as part of any redevelopment of the site:

- Consideration of a grade separated pedestrian link across Stirling Highway to the Mosman Park train station as an inherent part of the proposed development design; and
- Opportunities to incorporate public car parking accessed from Ferrier Lane as part of any redevelopment proposal.

#### 6.2.4.4 No. 42 (Lot 4) Glyde Street

The site comprises one (1) property of moderate development size with frontage to Glyde Street and Waite Lane and is characterised by a stand of significant trees adjacent to the Glyde Street frontage. As a group, the stand of significant trees represent the Botanical Heritage of the Town Centre area and the landscape vision for the Town Centre. The following matter should be considered as part of any redevelopment of the site:

Retention/protection of the existing stand of significant trees adjacent to the Glyde Street frontage.



#### 6.2.4.5 No. 594 (201) Stirling Highway

The site comprises one (1) property, irregular in shape and of moderate development size with frontage to Stirling Highway, Stuart Street, and Ferrier Lane. The Stirling Highway frontage is subject to road widening and is also part of the Stirling Highway Character Retention Area.

Ferrier Lane runs from Glyde Street parallel to Stirling Highway until it abuts the southern boundary of the subject site at which point it makes a right angle turn to head east parallel to Glyde Street. Ferrier Lane therefore does not extend through to Stuart Street and as a consequence does not link with Haydock Lane on the northern side of Stuart Street. The following matter should be considered as part of any redevelopment of the site:

- Provision of a laneway or similar, connecting Ferrier Lane and Stuart Street as part of redevelopment of the site.

#### 6.2.5 Downtown Character Retention Areas

Two (2) Character Retention Areas are identified within the Downtown Precinct Plan as follows:

- Stirling Highway Character Retention Area
- Glyde Street Character Retention Area

These Character Retention Areas comprise historic strip shops which positively contribute to the streetscapes of Stirling Highway and Glyde Street and provide a connection to the historic character of Downtown. Retention of all or a significant portion of these historic streetscape elements is strongly encouraged as part of any redevelopment of properties within the Character Retention Areas.

A minimum 5m (measured from the existing road reservation boundary) of the existing building is to be retained. No new built form (other than is required to maintain structural integrity of the retained portion of the original building as determined by the Council) is permitted within this area.

In support of redevelopment proposals that demonstrate retention of these elements the Town:

1. Will consider varying standards to facilitate design flexibility to achieve retention of the desired elements; and/or
2. Will consider a bonus where retention of the desired elements is achieved.

The Stirling Highway Character Retention Area is only applicable should current road widening proposals for Stirling Highway and the Stirling Highway/Glyde Street intersection be removed.

Should these requirements remain in place, development standards in this area will revert to the default standards applicable under the Town of Mosman Park Planning Framework with no variations or bonus opportunities related to retention of the above elements available.

### 6.3 Saint Leonard's Green Precinct

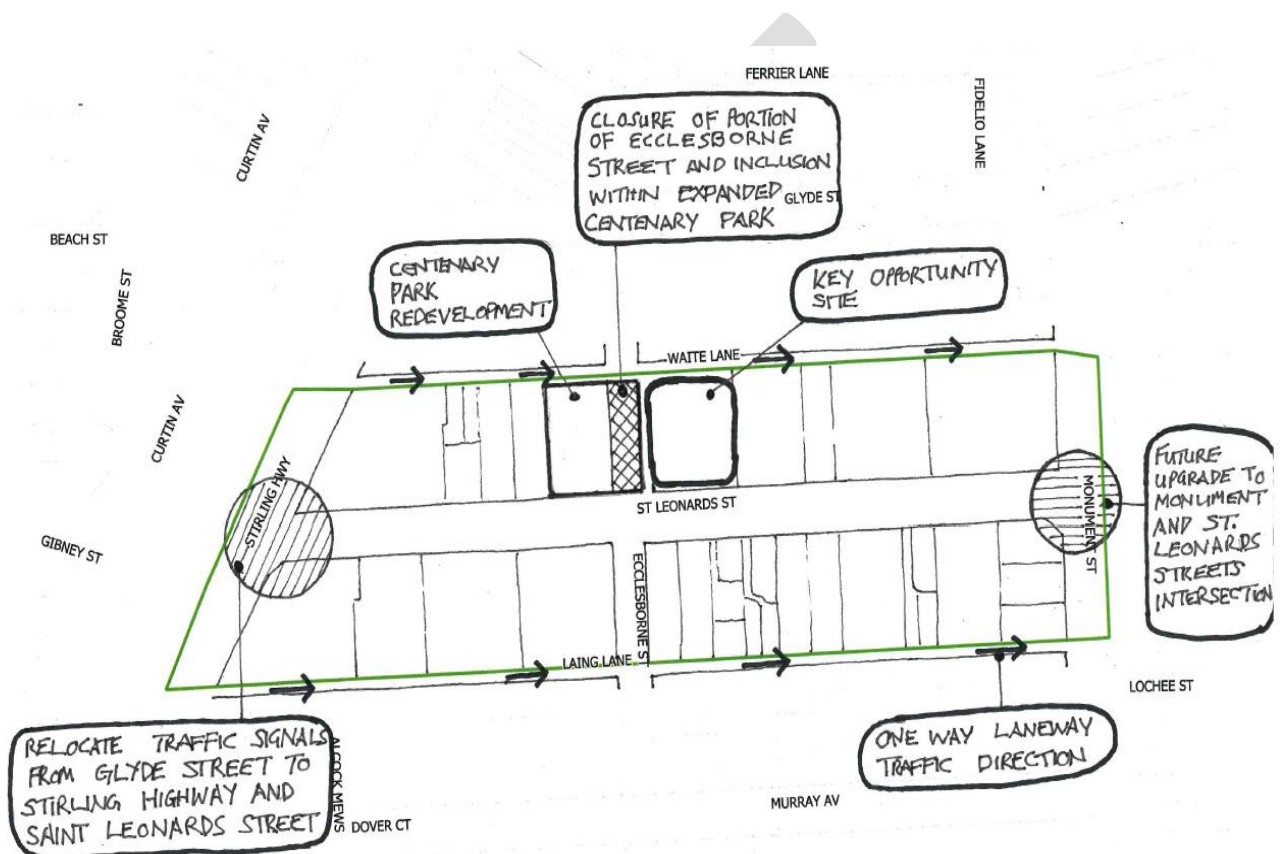


Figure 3: Saint Leonard's Green Precinct Plan

#### 6.3.1 St Leonard's Green Character Statement

An evolution of the existing character defined by historic low scale apartment development sited within well landscaped open space. Regardless of scale, new predominantly residential development is characterised by well vegetated and generous street setbacks and architecture continuing the historic precinct theme of simple building form, bold horizontal elements and quirky architectural detail.

Laneways are transformed into mews streets with modestly scaled built form and landscaping reflecting the 'green' character of the precinct. This theme is continued in the public realm with the expansion of Centenary Park and the street level



activation of key locations within the precinct successfully melding with their landscaped setting.

### 6.3.2 St Leonard's Green Design Guidelines

**TABLE 4: ST LEONARD'S GREEN PRECINCT DESIGN GUIDELINES**

Design Guide Line		Guide Line Intent
DGL 6.3.2.1	Buildings shall be setback a minimum of 6m from all street boundaries except laneway streets.	To maintain existing precinct character and facilitate sufficient provision of deep soil area to support large tree growth/planting and an expansive area of soft landscape/green canopy cover to be visible from public realm.
DGL 6.3.2.2	Buildings shall be setback a minimum of 2m from a laneway boundary.	To enable provision of landscaping in the Laneway streetscape.
DGL 6.3.2.3	Building facades to a street frontage shall include an emphasis on rectangular and linear elements.	To reflect historic precinct built form and promote cohesive precinct character.
DGL 6.3.2.4	Masonry and timber finishes shall be in a shade of white or alternatively a pale warm tone.	To reflect historic precinct built form and promote cohesive precinct character.

### 6.3.3 St Leonard's Green Design Guidance

**Table 5: ST LEONARD'S GREEN PRECINCT DESIGN GUIDANCE**

Design Guidance		Guidance Intent
DG 6.3.3.1	Any garage structure located adjacent to and accessible from a Laneway and separate from main building(s) should seek to compliment the architectural treatment of the main building.	To promote cohesive and integrated design for all development within the precinct.
DG 6.3.3.2	Buildings should express an elegant and uncluttered simplicity of design by avoiding applied decoration and by utilising bold structural elements.	To reflect historic building form in future development and promote cohesive precinct character.
DG 6.3.3.3	Building openings should be ordered and either grouped or paired together.	To reflect historic building form in future development and promote cohesive precinct character.
DG 6.3.3.4	Buildings should seek to respond to climate through the use of breezeways, shade	To promote a design response to climate that is consistent

	structures, etc. Where climate responsive design elements are proposed, these should be strongly expressed as part of the architectural language of the building.	with the desired building form and precinct character.
DG 6.3.3.5	The use of street frontage fencing is discouraged. Where fencing along the street frontage is proposed, this should be visually permeable and a maximum height of 1.2m above ground level.	To promote integration of landscaping within building setback areas and street verges as a preference and to maintain a visual connection to landscape with the public realm.
DG 6.3.3.6	Self-finishing materials such as stone, brick and timber are encouraged as a contrast to rendered finishes.	To reflect historic building form in future development and promote cohesive precinct character.

### 6.3.4 St Leonard's Green Precinct Key Sites

A key opportunity site is identified within St Leonard's Green which arising from its inherent characteristics may provide an opportunity to achieve additional outcomes in support of the vision for the Mosman Park Town Centre area.

Items required to be considered as part of any development proposed for this site are identified following. If these matters are addressed to the satisfaction of the Town as part of any development proposal they will be considered to satisfactorily meet the "Development Incentive 11" requirements of Part D of Local Planning Policy 15 (LPP 15) enabling consideration of possible development bonuses in accordance with LPP 15.

#### 6.3.4.1 No. 11 (Lot 500) St Leonard's Street

The site comprises a large development site (2276m<sup>2</sup>) with frontage to St Leonards Street, Ecclesborne Street and Waite Lane. It overlooks Centenary Park and is characterised by a large mature tree within the Ecclesborne Street frontage near the intersection with Waite Lane. The potential to close portion of Ecclesborne Street and incorporate into an expanded Centenary Park has been identified and any future redevelopment of the site should be considered within this context.

The following matters should be considered as part of any redevelopment of the site:

- Retention/protection of existing mature tree adjacent to the Waite Lane/ Ecclesborne Street boundary;

- Inherent design measures to promote activated land use at ground level overlooking Centenary Park.

## 6.4 Parkway Precinct

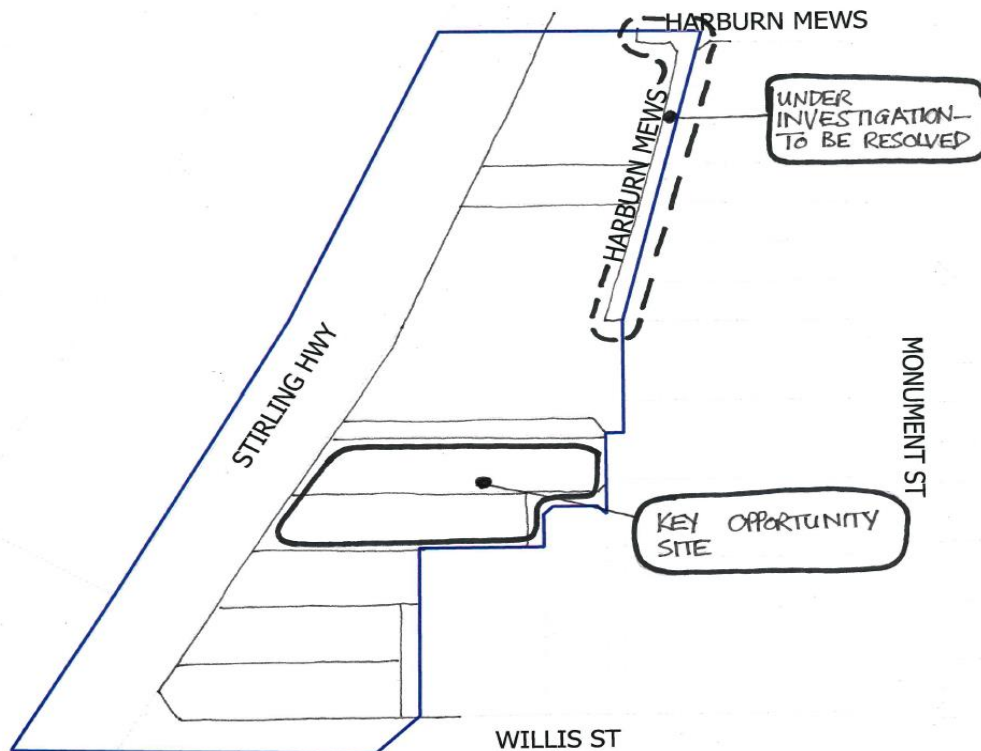


Figure 4: Parkway Precinct Plan

### 6.4.1 Parkway Precinct Character Statement

A highly visible commercially focused location comprising contemporary, simple and elegant building form with a high degree of transparency and an abundance of trees and landscape along Stirling Highway. Built form provides a well-defined edge to the public realm while the impacts of building bulk, scale, overshadowing and overlooking upon neighbouring precinct are moderated through careful design.

### 6.4.2 Parkway Design Guidelines

TABLE 6: PARKWAY PRECINCT DESIGN GUIDELINES

Design Guide Line	Intent
DGL 6.4.2.1	A minimum setback to the Stirling Highway frontage of 2m shall apply to enable provision of landscaping with the exception that additional setback shall be for large trees to be sited between the Stirling Highway boundary and
	To facilitate provision of a landscaped 'green parkway edge' to Stirling Highway and improved amenity for pedestrians.

	proposed development in accordance with the deep soil area requirements contained within the R-Codes Volume Two - Design WA.	
DGL 6.4.2.2	No fencing shall be permitted along the Stirling Highway frontage.	To facilitate maintenance of an 'open' aspect from Stirling Highway and cohesive precinct character.
DGL 6.4.2.3	Buildings shall be setback a minimum of 2m from a Laneway boundary to be measured from the existing boundary or in the instance where widening is proposed from the required widened Laneway boundary.	To enable provision of landscaping in the Laneway streetscape.
DGL 6.4.2.4	The ground floor of all development shall be aligned parallel with the Stirling Highway boundary.	To facilitate a cohesive frontage presentation to Stirling Highway.
DGL 6.4.2.5	The ground floor of all development shall be designed to include a glazed frontage of a minimum of 70% of the façade area.	To present a commercial business front and facilitate a cohesive presentation to Stirling Highway.
DGL 6.4.2.6	Buildings shall be a minimum two storeys in height. For any buildings proposed with more than two storeys, the third storey and above shall be setback from the rear and side boundaries in accordance with the standards contained in Local Planning Policy 15 for non-residential development or the R-Codes Volume Two - Design WA for residential use.	To facilitate establishment of a well-defined edge to the public realm while minimising impacts from building bulk and scale.
DGL 6.4.2.7	Where buildings also front a secondary street (Laneway) and are proposed with more than two storeys, the third storey and above shall be setback an additional 1.5m behind the minimum ground and first floor setback requirement.	To minimise potential impacts from building bulk, scale, overshadowing and overlooking at the interface with neighbouring precincts.

### 6.4.3 Parkway Precinct Design Guidance

**TABLE 7: PARKWAY PRECINCT DESIGN GUIDANCE**

	<b>Guidance</b>	<b>Intent</b>
DG 6.4.3.1	The building aesthetic should be designed to be bold and visually discernible from Stirling Highway, modern and elegantly formed. Storey heights should be expressed horizontally for delineation of proportion.	To promote development of contemporary, simple and elegant building form that presents a consistent precinct character.

DG 6.4.3.2	Roof form should be flat or shallow pitched. Where residential or mixed use building is proposed, the roof may be creatively designed to express the residential use.	To promote development that reflects a consistent precinct character.
DG 6.4.3.3	An upper floor should contain openings to enable passive surveillance of the public realm. Where a building overlooks a secondary street (Laneway) openings should be designed to minimise overlooking of adjoining precincts where possible.	To promote opportunities for passive surveillance as an inherent component of built form design while minimising potential impact on adjoining precincts.
DG 6.4.3.4	Signage should be carefully integrated to become an attractive component of the overall building design.	To promote consideration of signage as an inherent component of built form design.
DG 6.4.3.5	Colours and materials should express a modern aesthetic, such as through the predominant use of metal, painted and through colour cladding panels, glazing and portions of brick and timber.	To promote cohesive precinct character.

#### 6.4.4 Parkway Precinct Key Sites

A key opportunity site is identified within the Parkway Precinct which arising from its inherent characteristics may provide an opportunity to achieve additional outcomes in support of the vision for the Mosman Park Town Centre area.

Items required to be considered as part of any development proposed for this site are identified following. If these matters are addressed to the satisfaction of the Town as part of any development proposal, they will be considered to satisfactorily meet the “Development Incentive 11” requirements of Part D of Local Planning Policy 15 (LPP 15) enabling consideration of possible development bonuses in accordance with LPP 15.

##### 6.4.4.1 Lots 4 and 5 Stirling Highway

The site comprises abutting properties with a substantial combined area and frontage to Stirling Highway. No built form currently exists on the site although improvements include a driveway access and associated car parking area surrounded by grassed area and stands of mature trees. The site has been identified as one of two locations in the northern part of the Town Centre area that might offer potential for creation of an additional public open or civic space.

The following matter should be considered as part of any redevelopment of the site:

- Identification of a public space (Plaza/Park) incorporating existing mature vegetation/landscaping as part of any proposed development.

#### 6.4.5 Harburn Mews

The historic Harburn Mews laneway has legal but not physically usable access to the public street network. Consequently, existing properties with vehicle access unto Harburn Mews access the public road network by traversing existing car park areas of adjoining properties.

As part of redevelopment of these properties, provision of a vehicle access link from Harburn Mews to Stirling Highway as part of any development proposal is encouraged and would be strongly supported by the Town. Where such access is provided, the Town will support a development bonus as part of any development proposal.

#### 6.5 The Village Precinct

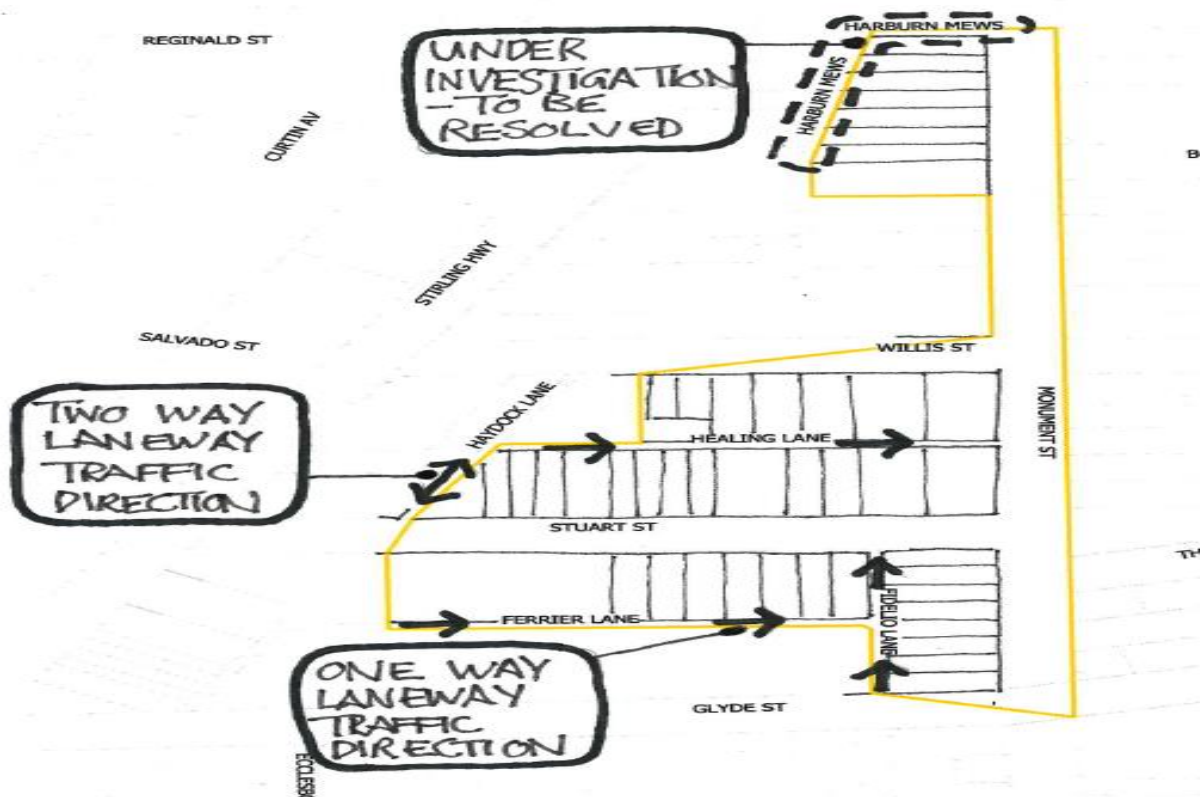


Figure 5: The Village Precinct Plan



### 6.5.1 The Village Precinct Character Statement

The past intertwines with the future in the Village, an interface between the active commercial context of the western side of the Town Centre and the predominantly lower scale residential areas to its east. It accommodates a combination of a range of residential types, commercial activity and mixed uses.

The precinct is distinguished by a continuity of well landscaped street and front gardens and an architecture that is inspired by the traditional character of the precinct through colours, materials, proportions and roof forms from previous eras.

### 6.5.2 The Village Design Guidelines

**TABLE 8: THE VILLAGE PRECINCT DESIGN GUIDELINES**

Design Guide Line		Intent
DGL 6.5.2.1	Buildings shall be setback a minimum 3m from the street frontage.	To support provision of area to accommodate deep soil areas sufficient to sustain small tree growth in support of desired precinct character.
DGL 6.5.2.2	'Cottage garden' landscaping, including provision for small trees, is to be provided within the street setback area with details to be delineated within a Landscaping Plan to be provided as part of the development application process.	To increase the quantity of publicly visible vegetation reflecting historic precinct character and to support a cohesive landscape across precinct area.
DGL 6.5.2.3	Fencing proposed along a street boundary shall be a maximum 1.2m in height above ground level and shall comprise timber picket or vertically aligned metal rails.	To maintain visibility of proposed landscape in support of a cohesive precinct character.
DGL 6.5.2.4	Within that part of the Village precinct zoned Commercial R-AC3 within Local Planning Scheme 3, above 3 storeys (4+ storeys) all building shall be setback a minimum of 4m from all boundaries.	To minimise/reduce building bulk as seen from street level by creating podium development above 3 storeys.
DGL 6.5.2.5	Buildings shall be setback a minimum of 2m from a Laneway boundary to be measured from the existing boundary or in the instance where widening is proposed from the proposed widened Laneway Boundary.	To enable provision of landscaping in the laneway streetscape.
DGL 6.5.2.6	Buildings three(3) storeys or less shall incorporate a pitched roof element that is visible from the adjacent street(s).	To reflect historic precinct character where roofs are visible from street level. Where new development results in roofs that are no

		longer visible from street level (above 3 storeys) this guideline is not applicable.
DGL 6.5.2.7	Buildings fronting a public street shall include a minimum of two (2) of the following materials in a compatible and complimentary design approach; painted textured render, weatherboard, face brick or stone.	To support cohesive precinct character.

### 6.5.3 The Village Design Guidance

**TABLE 9: THE VILLAGE PRECINCT DESIGN GUIDANCE**

Guidance	Intent	
DG 6.5.3.1	Those parts of a building fronting a public street should provide an interesting elevational treatment by incorporating elements designed to enhance detail and visual richness such as decorative fretwork, screens, mouldings and struts.	To enliven streetscapes by encouraging interesting elevational treatment for buildings fronting the public realm.
DG 6.5.3.2	Brick walls should be finished in a cream, buff, red or like colour while detailing such as railings, arches or canopies should be highlighted in black or white.	To reflect historic precinct characteristics and colour palettes.
DG 6.5.3.3	Roofs should be either metal in a light colour or roof tiles in a red, brown or like colour.	To reflect historic precinct characteristics and colour palette.

### 6.6 The Churches Precinct

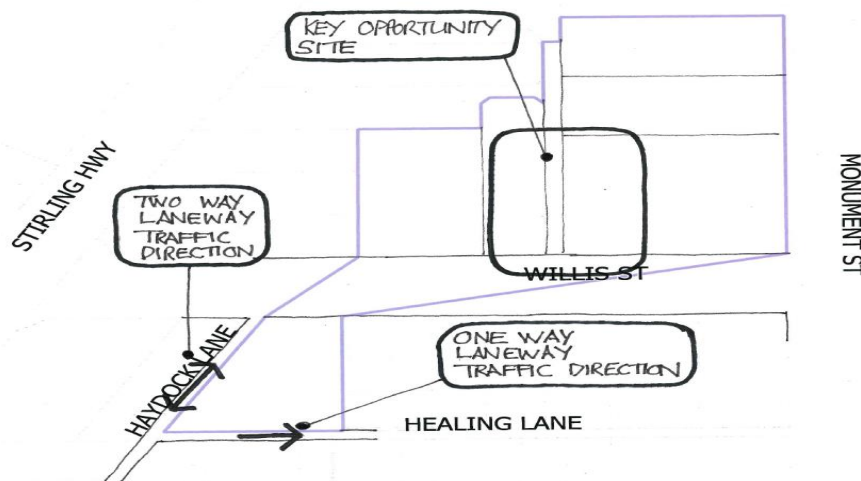


Figure 6: The Churches Precinct Plan



### 6.6.1 The Churches Precinct Character Statement

An enclave distinguished by a collection of ecclesiastical buildings and newer development which respects the historic built form by maintaining generous landscaped space between buildings, utilising a strong and simple pitched roof along with a predominance of brick and timber detailing. New buildings are formal with strong geometric forms, symmetry, repetition and axial vistas.

### 6.6.2 The Churches Precinct Design Guidelines

**TABLE 10: THE CHURCHES PRECINCT DESIGN GUIDELINES**

	<b>Design Guide Line</b>	<b>Intent</b>
DGL 6.6.2.1	Buildings shall be detached, be designed so as to be sited within a formal landscaped and open context and align with street boundaries. Existing significant trees are to be retained within the formal landscaped open areas between buildings.	To reflect historic precinct character enabling three dimensional building form to be visible from the public realm and retain existing significant trees.
DGL 6.6.2.2	Any fencing proposed along a street frontage shall be a maximum of 1.2m above ground level and comprise vertically aligned precincts or railing.	To facilitate a clear view of built form within its open and landscaped setting.
DGL 6.6.2.3	Built form shall be designed to show a simple solid and regular shaped form with architectural elements repeated on street elevations at different scales.	To reflect historic precinct character and ensure a cohesive precinct design approach.
DGL 6.6.2.4	Openings in buildings shall comprise narrow 'punched' openings and be vertically proportioned with visual design richness further included either as part of main built form, such as an arch with brick surrounds/bands, or via applied detail such as timber fretwork.	To reflect historic precinct character and to ensure a cohesive precinct design approach.
DGL 6.6.2.5	Building roofs shall comprise a minimum pitch of 30 degrees.	To reflect historic precinct character and ensure a cohesive precinct design approach.
DGL 6.6.2.6	Building wall material shall be either red brick, limestone, painted render or timber. Where walls are proposed to be painted they shall be painted in light and warm colours/tones only and roofs shall comprise either light coloured metal cladding or red-brown tiles.	To reflect historic precinct character and ensure a cohesive precinct design approach.

### 6.6.3 The Churches Precinct Design Guidance

**TABLE 11: THE CHURCHES PRECINCT DESIGN GUIDANCE**

	<b>Guidance</b>	<b>Intent</b>
DGL 6.6.3.1	Proposed built form should reflect the heritage character of the precinct by utilising geometrical and symmetrical form, fine detail and a palette of materials and colours reflective of existing built form.	To reflect historic precinct character.

### 6.6.4 The Churches Precinct Key Sites

A key opportunity site is identified within The Churches which arising from its inherent characteristics may provide an opportunity to achieve additional outcomes in support of the vision for the Mosman Park Town Centre area.

Items required to be considered as part of any development proposed for this site are identified following. If these matters are addressed to the satisfaction of the Town as part of any development proposal, they will be considered to satisfactorily meet the “Development Incentive 11” requirements of Part D of Local Planning Policy 15 (LPP 15) enabling consideration of possible development bonuses in accordance with LPP 15.

#### 6.6.4.1 No. 22 (Lot 11) Monument Street and Lots 12 and 103 Willis Street

The site comprises two lots and the undeveloped portion of a third all under the same ownership. It is characterised by the lack of any existing building form and an existing landscape incorporating a Community Garden which includes stands of mature trees. The adjacent verge has also been planted/landscaped and contains a significant mature tree. The following matter should be considered as part of any redevelopment of the site:

- Identification of a public space (plaza/park) incorporating existing mature vegetation/landscaping as part of any proposed development.

## DOCUMENT CONTROL

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## REVISION RECORD

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<b>17/09/2019</b>	<b>1</b>	<b>First Version</b>		<b>EMPRS</b>

DRAFT